

ORDINARY MEETING	6
28 APRIL 2010	

PUBLIC HOUSING PROJECTS AT PALM COVE, TRINITY PARK AND EARLVILLE - ASSESSMENT AGAINST THE RELEVANT PLANNING SCHEME PROVISIONS

Leon Doutre: 8/20/1: #2526816

RECOMMENDATION

That Council notes the report.

EXECUTIVE SUMMARY

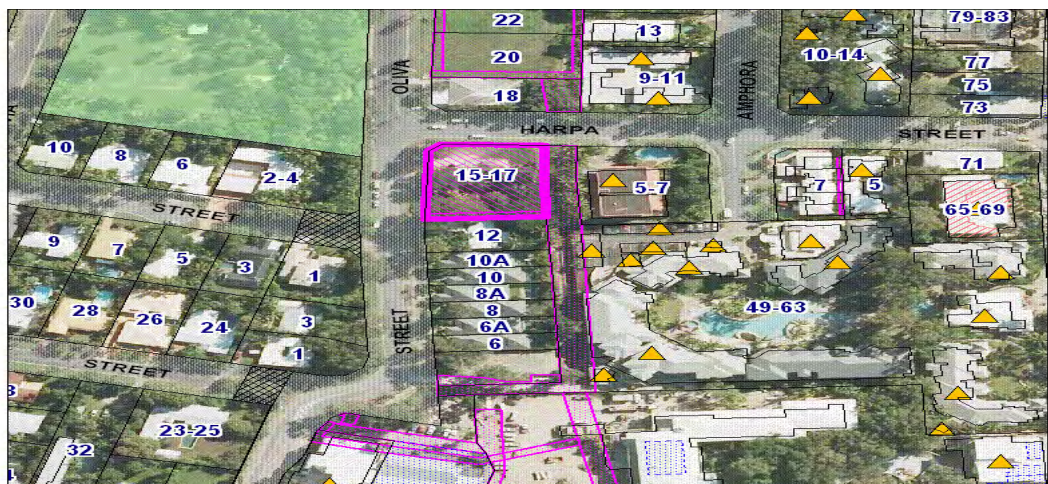
At the Ordinary Meeting held at Gordonvale on 31 March 2010, Council noted an Issues Paper on the Provision of Social Housing in the Cairns Region. As part of the resolution, Council requested a report on the specific issues with the public housing sites at Palm Cove, Trinity Park and Earlville. This report finds that the public housing projects are substantially in compliance with the respective planning schemes. The most notable departure from Council's planning provisions relates to on-site car parking provision and non compliance with Council's nominated desirable vernacular attributes associated with tropical design and appearance.

Introduction

The three public housing projects have been assessed against the applicable planning provisions as follows:

15 – 17 Harpa Street, Palm Cove

The proposed Public Housing development is located at 15-17 Harpa Street, Palm Cove, on land described as Lot 65 on RP868442.



The proposal involves the establishment of eighteen (18) two-bedroom multiple dwelling units on the site. The site has previously been subject of a development application for multi-unit housing (8/7/195). That application also encompassed the adjoining allotment at 12 Oliva Street. The previous application proposed a four storey building and basement car park multi-unit housing development, consisting of 28 units. The application was assessed against the requirements of CairnsPlan 2005 and approved by Council in November 2005 (and subsequently lapsed on 15 November 2009).

The proposed public housing project is of similar scale to the approved development, being of similar site coverage and four storeys in height. A copy of the submitted plans and letter are contained in Appendix 1. An assessment of the project against the relevant codes under CairnsPlan 2009 is provided below:

Tourist & Residential Planning Area: Code Assessable

Tourist & Residential Planning Area Code

Residential Density

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P1 Site Population Density must be compatible with the desired development outcomes of this planning area.	A1.3 The site population density is not greater than 400 persons per hectare	Complies – Site population density is approximately 280 persons per hectare.

Built Form

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P2 The height of buildings must be consistent with the desired character of the area and must not adversely affect the amenity of the area.	A2.3 Buildings are no more than 11.5 metres in height.	Marginally exceeds Acceptable Measure, but complies with the Performance Criteria: The proposed development slightly exceeds the specified height limit of 11.5m (being 12m). However, the scale of development is consistent with neighbouring development. Numerous four-storey developments exist in the immediate vicinity, including the adjacent property on Harpa Street.

Character & Community Design

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P3 Development does not adversely affect the amenity of: a) the planning area; and b) adjoining land uses.	A3.1 No acceptable measures are specified.	Complies – Development is consistent in scale with surrounding development.
P4 Buildings, structures and associated services are responsive to the natural features and constraints of the land.	A4.1 The design of the buildings, structures and associated services takes into account: a) established trees; b) significant vegetation; c) ecological values; d) slope; e) waterways; and A4.2 On land steeper than 25% (1 in 4) alternative construction methods to slap on ground are utilised.	Complies – The block in question is completely flat and has been previously cleared. It is noted that the building encroaches into a drainage easement at the rear of the site.

<p>P5 Buildings and structures are:</p> <p>a) responsive to the tropical climate by taking into account prevailing breezes and solar orientation; and</p> <p>b) designed to minimise energy consumption.</p>	<p>A5.1 Buildings and structures are sited having regard to</p> <p>a) significant views and vistas;</p> <p>b) predominant breezes;</p> <p>c) slope;</p> <p>d) solar orientation.</p>	<p>Complies – The building has been orientated so as to achieve greater setbacks to both road frontages. The predominant breezes being from the south-east will likely be partially negated by the adjacent four storey development along the esplanade.</p>
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Multiple Dwelling and Holiday Accommodation Code

Site Requirements

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P1 The premises has sufficient area and dimensions to accommodate:</p> <p>a) the buildings; and</p> <p>b) other structures; and</p> <p>c) open space; and</p> <p>d) car parking; and</p> <p>e) vehicular access; and</p> <p>f) pedestrian access; and</p> <p>g) landscaping; and</p> <p>h) recreational facilities.</p>	<p>A1.1 Tourist and Residential Planning Area : Minimum Area = 600m².</p> <p>A1.2 The site has a minimum Main Road frontage of 15 metres</p>	<p>Complies – Site is 1565m².</p>

Built Form

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE										
<p>P2 To retain the existing character of the residential suburbs of Cairns, buildings are designed to:</p> <p>a) maximise the separation between buildings; and</p> <p>b) provide adequate landscaping and open space around buildings for screening and recreational opportunities for the residents and visitors of the site; and</p> <p>c) achieve a balance between parking, driveways, service areas and landscaping and building; and</p> <p>d) promote through breeze, natural light, vistas and visual lines; and</p> <p>e) reduce the bulk of buildings.</p>	<p>A2.1 The maximum site cover is 40%; and</p> <p>A1.4 The gross floor area shall not exceed the following:</p> <table border="1" data-bbox="576 1171 922 1413"> <thead> <tr> <th>Planning Area</th> <th>Maximum GFA</th> </tr> </thead> <tbody> <tr> <td>Residential 2</td> <td>0.8 x site area</td> </tr> <tr> <td>Residential 3</td> <td>1.2 x site area</td> </tr> <tr> <td>Tourist & Residential</td> <td>1.2 x site area</td> </tr> <tr> <td>Other Areas</td> <td>No acceptable measure specified</td> </tr> </tbody> </table>	Planning Area	Maximum GFA	Residential 2	0.8 x site area	Residential 3	1.2 x site area	Tourist & Residential	1.2 x site area	Other Areas	No acceptable measure specified	<p>Complies – Though no accurate numbers for GFA are supplied the development is capable of satisfying the Performance Criteria.</p>
Planning Area	Maximum GFA											
Residential 2	0.8 x site area											
Residential 3	1.2 x site area											
Tourist & Residential	1.2 x site area											
Other Areas	No acceptable measure specified											
<p>P3 The development achieves tropical character and a Cairns vernacular through:</p> <p>a) the use of structural elements and building materials of varying scales and textures; and</p> <p>b) variations in exterior colours; and</p> <p>c) variations in the size and patterning of windows; and</p> <p>d) use of awnings and other sun protection devices.</p>	<p>A3.1 No acceptable measures are specified.</p> <p><i>See Figure 1 below for an example</i></p>	<p>Does Not Comply – From the plans provided there would appear to be little in the built form that is representative of tropical vernacular as the building is almost entirely concrete and block work – though no details regarding colours or finishes have been provided.</p>										

<p>P4 The building is designed and sited to ensure adequate natural daylight in habitable rooms and principal open space areas, to allow residents and visitors enjoyment during the majority of the daylight hours.</p>	<p>P4.1 No acceptable measures are specified</p>	<p>Complies – The recess on the southern side should help the penetration of sunlight into the majority of dwelling units.</p>
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PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P5 The building bulk is reduced by a combination of:</p> <ul style="list-style-type: none"> a) balconies; and b) recesses in the vertical plane; and c) variation in building form; and d) roof features; and e) the use of curves, steps, recesses, projections or variation in plan and elevation, including articulated or separated balconies. 	<p>A5.1 Building bulk and form is reduced by:</p> <ul style="list-style-type: none"> a) the length of elevation does not exceed 15 metres, with a minimum change in plane in plan or elevation of 1.5 metres; and b) there is a minimum distance of 7 metres between adjoining residential buildings within the site; and c) the use of curves, steps, recesses, projections or variations in plan and elevation; and d) the provision of articulated or separated balconies; and e) variation in building colours, materials and textures; and <p>A5.2 Rooflines are broken up and include features that reduce the overall bulk and add scale (such as cascading roof levels, gables, skillions or variations in pitch); and</p> <p>A5.3 The length of any continuous eaves line does not exceed 16m.</p>	<p>Complies – Some level of articulation is provided by the balconies along the western elevation and the recess on the southern elevation. However, the southern end of the building does show a stark three storey rise.</p> <p>The elevation to Harpa Street shows the building as only three storey and does appear to have some small measure of articulation provided by the balconies facing to the east and west. Twin patios also face out toward Harpa Street.</p>
<p>P6 The overall scale, proportions and rhythm of the development:</p> <ul style="list-style-type: none"> a) responds to traditional subdivision patterns; and b) is sympathetic to the traditional streetscape pattern. 	<p>A6.1 No acceptable measures are specified.</p> <p><i>See Figure 2 below for an example</i></p>	<p>N/A</p>

Figure 1 – Illustration of Desirable Character Attributes for Multiple Dwellings

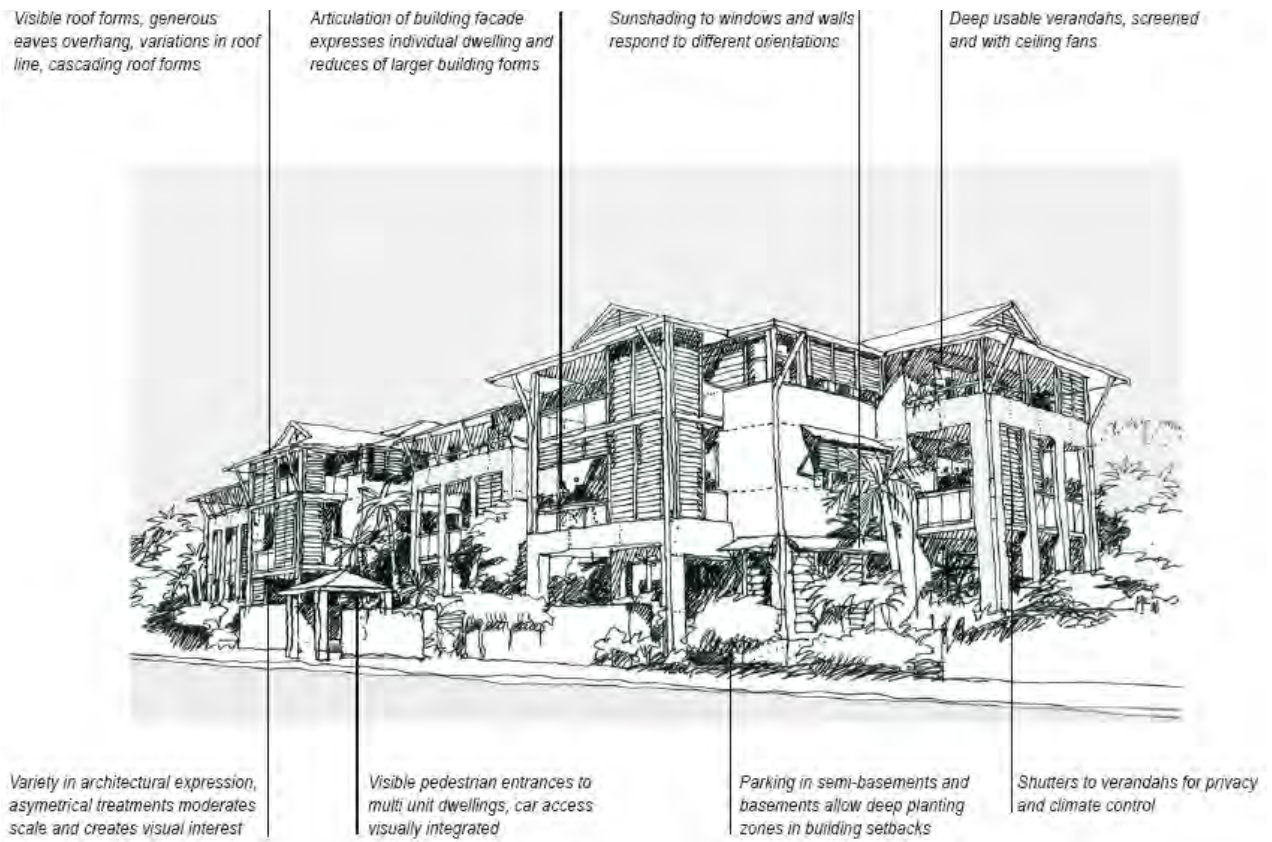
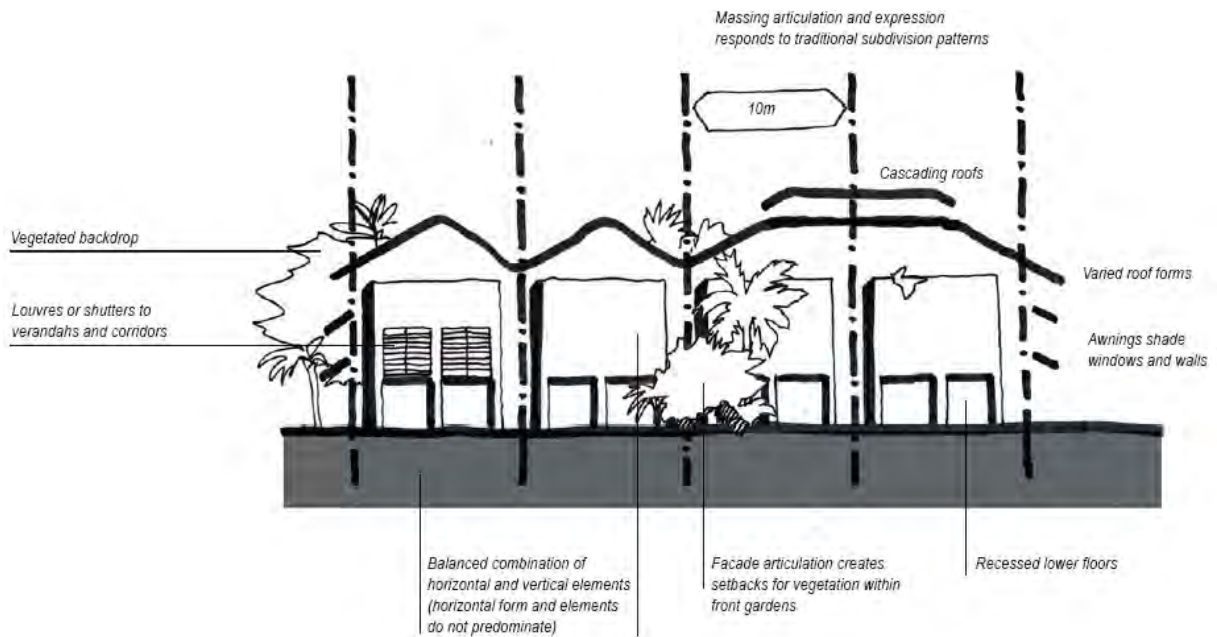
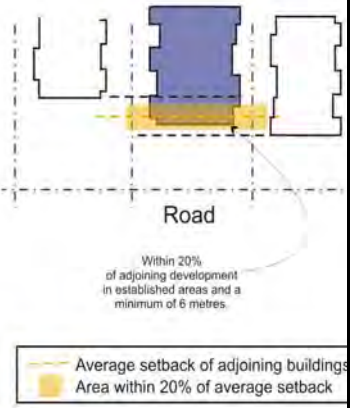
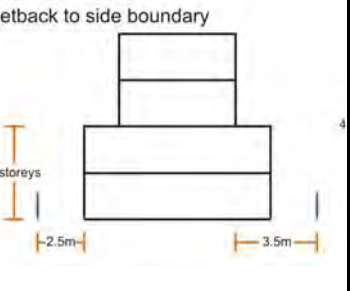


Figure 2 – Scale, Proportions and Rhythm of the Building Reflects the Dominant Streetscape

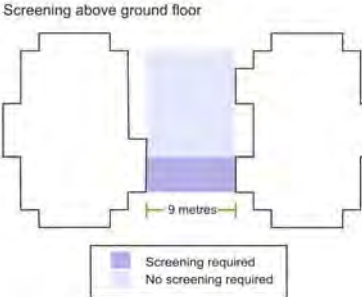


PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P7 Buildings shall be sited such that:</p> <ul style="list-style-type: none"> a) the setback from street frontages is consistent with the established streetscape pattern; and b) the setback from side and rear boundaries retains daylight access and privacy for adjoining properties; and c) the setback from all boundaries is sufficient to allow areas of deep planting; and d) the setback from street frontages provides for the desired streetscape pattern. 	<p>A7.1 Buildings are setback 15 metres to the Cairns Esplanade; and</p> <p>A7.2 For buildings in other locations, building setback from main road frontage is:</p> <ul style="list-style-type: none"> a) a minimum of 6 metres; and b) in established areas within 20% of the average setback of adjoining development; and <p>Figure 3 Example of front setback.</p> <p style="text-align: center;">Setback to front boundary</p>  <p>A7.3 Regardless of clauses 7.4 and 7.5 below, for the purposes of reducing overshadowing, the setback along the southern and western boundary shall be one third of the height of the building, measured to the eaves; and</p> <p>A7.4 The rear boundary setback is a minimum of 4 metres.</p> <p>A7.5 The side boundary setback is:</p> <ul style="list-style-type: none"> a) a minimum of 2.5m for the first 2 storeys; and b) a minimum of 3.5 m for the next 2 storeys, and <p>Figure 4 Example of Side Setback.</p> <p style="text-align: center;">Setback to side boundary</p> 	<p>Complies – As discussed, the building has been deliberately sited so as to maximise the setbacks to both road frontages. As a result, the building has reduced setbacks to the side boundaries.</p> <p>The building is setback approximately 7m from Harpa Street and 5m from Oliva Street.</p> <p>The outermost projections lie within 1.5m of the rear boundary. This is for the parking structure. The remainder of the building is setback in accordance with A7.5.</p>

	Note: All setbacks are measured in plan view.	
P8 Services structures and mechanical plant (including air conditioners and split system air conditioners) are screened or otherwise incorporated as part of the building form.	<p>A8.1 Services structures and mechanical plant are not visible from:</p> <ul style="list-style-type: none"> a) the street, and b) adjoining properties, and c) public open space; and <p>A8.2 Mechanical plant is not located:</p> <ul style="list-style-type: none"> a) on balconies or adjacent to other liveable areas; and b) near multiple reflective surfaces such as walls and eaves; and <p>A8.3 Services and mechanical plant are located in building recesses or service areas such as parking and driveway areas.</p>	Unknown – It is not known whether air-conditioning is proposed. Normally a condition would be applied to the development for the provision of A/C screens.
P9 The development does not adversely impact on the natural environment and is designed to be sympathetic to natural constraints.	<p>A9.1 The siting and design of individual buildings within the development ensures:</p> <ul style="list-style-type: none"> a) the retention of existing mature vegetation on the site, with appropriate separation between vegetation canopy and root systems and buildings; or b) where selected removal of mature tree/s is required, suitable established tree/s are planted on site as a feature; and <p>A9.2 On land steeper than 25% (1 in 4) alternative construction methods to slab on ground are utilised.</p>	Complies – The site has previously been cleared.

Amenity

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P10 Easily accessible and functional communal landscape and recreation area is provided for the benefit of residents.	<p>A10.1 A minimum of 35% of the site is communal landscaped and recreation area including:</p> <ul style="list-style-type: none"> a) at least 1/2 of the required landscaped area has a minimum dimension of 5m; b) deep planting to at least 1/3 of the required landscaped area, and c) paved and sealed areas do not exceed 1/3 of the required landscaped area; and <p>A10.2 For a development having 10 or more dwelling units at least 1 communal recreational feature is provided, including but not limited to a barbeque facility, swimming pool or children's play area.</p>	Unknown – No details on landscaping have been provided.

<p>P11 Easily accessible and functional private outdoor living area is provided for each unit</p>	<p>A11.1 Each dwelling unit, has a private and functional outdoor living area:</p> <ul style="list-style-type: none"> a) having minimum dimensions of 3 metres; and b) directly accessed from internal living spaces (not bedrooms). 	<p>Complies – Each unit has a small patio or verandah.</p>
<p>A12 The development is sited and designed to minimise overlooking of other dwelling units within the development, and of residential buildings on adjoining properties.</p>	<p>A12.1 Where habitable rooms look directly at habitable rooms in residential buildings within 2m at ground floor level or within 9m above ground floor level, privacy is protected by:</p> <ul style="list-style-type: none"> a) fixed obscure glazing in any part of the window below 1.5m above floor level; or b) fixed external screens; or c) sill heights of 1.5m above floor level; or d) in the case of screening for the ground floor level, fencing to a height of 1.8m above ground level. <p>Figure 5 Example of when Screening is Required</p> 	<p>Complies – There would not appear to be any overlooking issues associated with the development. All units are orientated away from one another.</p>
<p>P13 The development addresses the road frontage to facilitate casual surveillance and to enhance the amenity of the streetscape.</p>	<p>A13.1 The building has balconies, windows and verandahs that face the street; and</p> <p>A13.2 Visual privacy is provided to units that face the street through:</p> <ul style="list-style-type: none"> a) screening; and b) planting; and <p>A13.3 Fences and walls to road frontages are limited to:</p> <ul style="list-style-type: none"> a) 1.2 metres in height if solid; or b) 1.5 metres if partially transparent; or c) 1.8 metres if 50% permeable. <p>Note: Alternative solutions will be considered for developments on State Controlled Roads and future State Controlled Roads.</p>	<p>Complies – Each unit contains a verandah or patio and they will generally be of a height such that there are minimal privacy issues from the street. No details of the treatment used on the screening of the balconies are provided.</p> <p>No details on the fencing proposed for the site has been provided.</p>
<p>P13 A secure storage area is provided for each dwelling unit.</p>	<p>A13.1 Secure storage areas:</p> <ul style="list-style-type: none"> a) are located to enable access by motor vehicle; and b) have a minimum space of 3.5m² per dwelling unit; and c) have a minimum height of 2.1 metres; and d) are weather proof; and 	<p>Does Not Comply – No provision for storage appears to have been made from the plans supplied.</p>

	<ul style="list-style-type: none"> e) are lockable; and f) are ARI 100 immune. 	
<p>P14 Service structures and mechanical plant (including air conditioners and split system air conditioners) are located or insulated such that dwelling units within the development, and in residential buildings on adjoining properties are not adversely affected by the noise source.</p>	<p>A14.1 Service structures and mechanical plant are :</p> <ul style="list-style-type: none"> a) Screened and baffled; or b) Otherwise incorporated into the building form to maintain and uphold the amenity of dwelling units within the development, and of residential buildings on adjoining properties. 	See P8
<p>P15 Refuse storage areas are:</p> <ul style="list-style-type: none"> a) convenient and accessible to residents and refuse collection services; and b) located and designed to mitigate adverse impacts: <ul style="list-style-type: none"> i) within the site; and ii) on adjoining properties; and iii) to the street. 	<p>A15.1 Refuse storage areas:</p> <ul style="list-style-type: none"> a) are located on site; and b) are sited and designed to be unobtrusive and screened from view from the street frontage; and c) are imperviously sealed roofed and bunded, and contain a hose down area draining to Council's sewer network; and d) are of a sufficient size to accommodate bulk (skip) bins; and e) have appropriate access and sufficient on site manoeuvrability area for refuse collection services. 	Complies – Provision has been made for a refuse storage area measuring 3m x 3.5m.

Energy Efficiency

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P16 Buildings are sited and designed to:</p> <ul style="list-style-type: none"> a) maximise cross-breezes through the site; and b) minimise solar heat loads; and c) promote breeze and natural light. 	<p>A16.1 Buildings are oriented on site to:</p> <ul style="list-style-type: none"> a) allow prevailing south-east and north-east breezes to penetrate the site; and b) maximise the exposure of individual dwelling units to prevailing and cross-breezes; and c) minimise exposure to western sun; and <p>A16.2 Individual dwelling units are planned for maximum natural ventilation and light. Every dwelling unit has:</p> <ul style="list-style-type: none"> a) access to prevailing breezes including external walls with openings in at least two different orientations to allow breeze paths within the dwelling unit; or b) partitioned walls, vents in or above doorways and internal louvers as required to ensure air movement through the dwelling unit; and c) access to a covered, outside area accessible to breezes for the drying of clothes. This area 	Complies – Units are not double banked, though limitations on the breeze occur due to the neighbouring development.

	<p>shall be sited such that it is not visible from public spaces.</p> <p>A16.3 Units are not located on both sides of a central corridor (i.e. not double banked).</p>	
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Parking and Access

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P17 Vehicle access and parking for residents, visitors and service providers:</p> <p>a) does not dominate the street frontage; and</p> <p>b) does not impact on the road network; and</p> <p>c) is safe and convenient; and</p> <p>d) is accessible.</p>	<p>A17.1 The road providing access has a minimum road reserve width of 20 metres; and</p> <p>A17.1 The location of visitor parking is easily identified from the street; and</p> <p>A17.2 Where access is gated:</p> <p>a) there is a safe pullover area within the property boundary to use an intercom or gain access; and</p> <p>b) queuing space outside the gated for 2 vehicles using and awaiting the use of the intercom / access; and</p> <p>c) a minimum of two parking spaces are provided outside of the gated area and within the property boundary, and.</p> <p>A19.2 Vehicle parking is:</p> <p>a) illuminated at night; and</p> <p>b) close to the dwelling units to be served.</p>	<p>Complies – Parking is located under the building and therefore does not dominate the streetscape. Units 1 and 2 should provide some passive surveillance for the parking area.</p>
<p>P18 Residents Vehicle access and parking</p>	<p>A19.3 Access to basement parking is ARI 100 immune; and</p> <p>A19.4 Ventilation and pump-out infrastructure for car-parking areas is ARI 100 immune.</p>	<p>Unknown</p>
<p>P19 Pedestrian access is prominent and safe.</p>	<p>A18.1 A well-lit, sealed footpath, of minimum width 1.2m, links the development with the on street works; and</p> <p>A18.2 The development has at least one prominent, well-lit pedestrian entry with clear visibility for public surveillance.</p> <p>A18.3 Pedestrian access to the site is:</p> <p>a) via a well-lit and clearly delineated shared traffic area for developments of 4 dwelling units or less; and</p> <p>b) via a well-lit path that is separate from the vehicle access for all other developments; and</p> <p>A18.4 Buildings, fences and landscaping are designed and lit so that they will not conceal a person.</p>	<p>Complies – Provision has been made for the footpath to the front boundary, though it is not known as to what lighting/finishes is involved.</p>
<p>P20 The design and location of vehicle access and parking:</p> <p>a) minimises impacts on adjoining dwellings and</p>	<p>A20.1 A minimum of 1 metre wide dense planted buffer is provided adjacent to any vehicle movement area.</p>	<p>Complies – Undercover parking will be largely obscured from the street and should result in minimal impacts to neighbouring developments.</p>

<p>compliments the streetscape; and</p> <p>b) reduces thermal radiation, minimises noise and lessens the visual impact of hardstand area.</p>	<p>A20.2 Vehicle access and parking is:</p> <p>a) oriented or screened to minimise reflection of car headlights and area lighting reflecting into the windows of habitable rooms and patios or balconies of dwelling units and adjacent dwellings / properties; and</p> <p>b) separated from windows to habitable rooms either by location, enclosure or screening to minimise noise and light disturbance and fume emissions.</p> <p>A20.3 Vehicle parking structures are designed such that they:</p> <p>a) are compatible with overall building design in terms of roof form, detail, material and colours; and</p> <p>b) are consistent with stipulated setbacks for the buildings, and</p> <p>c) do not dominate the streetscape.</p>	
<p>P21 Bicycle parking for visitors and residents provides;</p> <p>a) secure parking facilities; and</p> <p>b) convenient access; and</p> <p>c) safe access; and</p> <p>d) protected from the sun and rain.</p>	<p>A21.1. On-site bicycle parking shall be provided at a rate of:</p> <p>a) 1 space per 16 dwelling units for visitor parking; and</p> <p>b) 1 space per 4 dwelling units for residential parking; and</p> <p>A21.2 Parking shall be designed and located to:</p> <p>a) enable wheels and frame to be locked to the device without damaging the bicycle; and</p> <p>b) be located outside pedestrian movement paths; and</p> <p>c) be easily accessible from the road; and</p> <p>d) be arranged so that parking manoeuvres will not damage adjacent bicycles; and</p> <p>e) be protected from manoeuvring motor vehicles and opening car doors; and</p> <p>f) be well lit; and</p> <p>g) be protected from the weather.</p>	<p>Unknown</p>
<p>P22 Visitor parking remains accessible and useable to visitors at all times.</p>	<p>A22.1 Visitor car parking bays are not allocated to individual dwelling units; and</p> <p>A22.2 Visitor car parking is not gated or located behind security doors/gates; and</p> <p>A22.3 Visitor car parking is visible from the street frontage, clearly signed and delineated; and</p> <p>A22.4 Visitor car parking bays are</p>	<p>Does Not Comply – No provision for visitor spaces has been made</p>

	<p>not provided in a tandem arrangement; and</p> <p>A22.5 Visitor car parking bays are not located in front of private storage rooms allocated to individual units; and</p> <p>A22.6 Visitor car parking bays are not provided in parallel formation along a driveway.</p>	
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Public Art

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P23 Public art is provided that is visible and accessible to the general public and reflects the local character of Cairns through a variety of mediums.	A23.1 Public art is provided in accordance with Planning Scheme Policy – Public Art.	N/A

Vehicle Parking & Access Code

Vehicle Parking Numbers

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	
<p>P1 Sufficient parking spaces must be provided on the site to accommodate the amount and type of vehicle traffic generated by the development of the site, having particular regard to:</p> <ul style="list-style-type: none"> a) the desired character of the area in which the premises is located; and b) the nature and scale of the development; and c) accessibility to the premises; and d) the nature and frequency of public transport serving the area; and e) whether or not the development involves the retention of an existing building, particularly an identified historic building, and the previous requirements for car parking for the building; and f) whether or not the use involves the retention of other cultural heritage features or significant vegetation; and g) the different types of vehicles that visit the premises are adequately accommodated. 	<p>A1.1 The minimum number of parking spaces provided on the site is not less than the number prescribed in Schedule 1 to this Code for the particular development.</p>	<p>Does Not Comply – Schedule prescribes that the development should make provision for 27 car parking spaces. The project currently proposes 18 spaces of which 6 are disabled spaces.</p>

Comments

It is difficult to provide a full assessment of the impacts of the development on the neighbourhood given the very limited information provided. No assessment against the Landscaping Code is provided as no details on the landscaping were submitted. Many of the neighbouring developments in Palm Cove of similar scale have created attractive street frontages (particularly along Williams Esplanade) through extensive landscaping, centred around existing *Melaleuca* sp. If the project was an assessable development, Officers would seek further information on landscaping, or condition for landscaping to be employed to help soften the built form, mitigate any privacy issues and better integrate the project into vegetated character of the area. The main areas of non compliance relate to car parking and external appearance/vernacular.

Trinity Park

The proposed Public Housing development is proposed at 24-32 Harbour Drive, Trinity Park, on land located at Lot 1801 on SP165903.



The proposal involves the establishment of nineteen (19) two-bedroom units, configured as five (5) two-storey buildings on the site. A single access point is to link the development with Harbour Drive and will service nineteen (19) car parking spaces. The site is subject to a Court Consent Order for the Earl Hill South Special Facilities (17 April 1998) that permits the establishment of multiple dwelling units scattered on various sites throughout the estate, including the subject site. The original Special Facilities Zoning made provision for Lot 1801 to accommodate nineteen (19) dwelling units. There are a number of conditions in the Court Consent Order that directly relate to this parcel of land:

14. *The applicant is to provide bollards to prevent vehicle access, in accordance with Council's policy, between the proposed townhouses and the road stubs off Debbie Street, being Wendy Street and the unnamed road stub towards the western end of Debbie Street.*

15. *In respect to the proposed townhouse allotments, adjacent to existing properties of Debbie Street, a landscaped buffer for visual screening purposes is to be provided between the townhouse development and the existing residential allotments at the time of construction of the townhouses, all to the requirements and satisfaction of the Director of Planning and Development. All townhouse development shall be in accordance with the relevant provisions of section 2 Part D of the Planning Scheme for the balance of the City of Cairns.*

Council Officers responded to the statutory notification provided by Project Services on 22 March 2010. The proposal complies with Condition 14 in that no vehicular access is proposed through Wendy Street. A concrete pedestrian pathway is shown to provide linkage to the site to Debbie Street. It was recommended to Project Services that these links include appropriate ground level lighting so as not to detrimentally impact on the amenity of neighbouring properties while still facilitating appropriate crime prevention outcomes.

Condition 15 specifies that the development is subject to Part D - Section 2 of the Planning Scheme for the Balance of the City of Cairns (November 1996). A brief assessment of the proposal against these provisions is provided below:

Performance Criteria	Compliance
2.2.1 Site Layout Element 1	
<p>[a] Neighbourhood Integration PC1 The development is well connected into the neighbourhood through adequate vehicle and pedestrian links which maintain or enhance local accessibility.</p> <p>PC2 The development contributes to or enhances neighbourhoods of valued and definable character through layout, massing and detailing that integrates well with its surroundings.</p> <p>PC3 The design and layout of dwellings enhances existing streets, maintains amenity for adjacent residents, and provides a sense of address and privacy for residents. This may be achieved by:</p> <p>[i] ensuring that most dwellings front existing and proposed streets;</p> <p>[ii] minimising principal private open space in front of dwellings, as this often results in high fencing for privacy;</p> <p>[iii] amalgamating two or more residential lots for redevelopment so a central street or driveway can be created;</p> <p>[iv] using a mews street arrangement so that dwellings can front other dwellings where they cannot relate directly to a street;</p> <p>[vi] varying the alignment of accessways to provide visual interest and avoid a 'gunbarrel' effect;</p>	<p>Partially Complies - By providing the pathway linkages as discussed above, the development will go some way in integrating itself into both Harbour Drive and Debbie/Wendy Street. The layout of the units should result in the majority of outcomes being achieved. A private yard is provided for all units on the ground floor and the majority of common area on the site is available for passive surveillance.</p> <p>- 1.2m high fence to face Harbour Drive</p>

<p>[vi] arranging the development to minimise outlook over side boundaries by ensuring that main upper-storey windows face the street, interior or back of the development;</p> <p>[vii] locating the multi-storey component of a development towards the street to limit impact on the amenity of adjacent private open space.</p> <p>PC4 In larger scale new developments, varied dwelling types and forms are integrated along both sides of streets. This may be achieved by:</p> <p>[i] providing a similar type or mix of buildings on both sides of a street to generate a consistent streetscape;</p> <p>[ii] ensuring dwelling fronts face dwelling fronts, not backs, across a street;</p> <p>[iii] not locating large-scale strongly contrasting dwelling types across the street from each other. Major changes in dwelling types should occur across rear site boundaries.</p> <p>[b] Site features and constraints</p> <p>PC5 The development capitalises on views from the site, incorporates existing significant vegetation and site features, and overcomes site constraints such as steep slope, unstable soils, poor drainage or bushfire risk.</p> <p>PC6 The development retains buildings of identified conservation significance and buildings with efficient recycling potential.</p> <p>PC7 In areas with significant off-site noise, the development designed to minimise entry of noise and to limit the number of dwellings that are exposed to high noise levels.</p> <p>[c] Community safety</p> <p>PC8 The layout contributes to community and property safety, and to the security of the neighbourhood. This may be achieved by:</p> <p>[i] ensuring dwellings abutting streets and public open spaces have windows that enable surveillance of the area;</p> <p>[ii] minimising high fences and walls along streets;</p> <p>[iii] providing lighting, good visibility and surveillance of lanes;</p> <p>[iv] protecting private spaces from inappropriate use as public thoroughfares;</p> <p>[v] minimising obscured, overgrown and shrubby areas along streets and paths.</p> <p>[d] Energy and environmental management</p> <p>PC9 The layout contributes to energy efficiency of the development. This may be achieved by:</p> <p>[i] establishing a pleasant, safe and connected street network, supporting walking and cycling to public transport and local facilities;</p> <p>[ii] orientating habitable rooms to limit solar radiation and maximise circulation of breezes.</p> <p>PC10 The layout facilitates environmental management, for example by:</p> <p>[i] providing for infiltration of stormwater runoff by limiting paved surfaces and providing onsite retention basins;</p> <p>[ii] retaining existing large trees where practicable and providing space for additional large trees. A setback of 4 metres to buildings is a useful guide.] Car parking</p> <p>PC11 The layout provides for car parking and garages that do not dominate the development or street</p>	<p>- Overlooking issues largely mitigated by building orientation. Building closest to southern and western boundaries is limited to a single storey.</p> <p>- Private yards provided for each unit are separated by 1.8m timber paling fence.</p> <p>- Development in the immediate area is generally considerable in scale. Streetscape is dominated by very large single detached dwellings with reduced side setbacks.</p> <p>- Site in question is completely flat and contains no significant vegetation. Legal point of discharge for the development is Harbour Drive</p> <p>- Development is orientated to reduce impacts from parking areas though some units will still provide passive surveillance of these areas.</p> <p>- Officers have requested that Project Services provide an appropriate lighting level to the pathways and driveways on the site to a level where there are minimal impacts to the amenity of neighbouring properties but still facilitate crime prevention outcomes.</p> <p>- Pedestrian link through to Wendy Street should help the connectivity of the site through to local parks (Debbie Street) and public transport links.</p> <p>- Car parking is contained in the middle of the site and will</p>
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<p>frontage. This may be achieved by:</p> <ul style="list-style-type: none"> [i] locating parking well within the development; [ii] breaking up large parking areas with trees, buildings or different surface treatments; [iii] providing car parking underground or in semi basements where practicable; [iv] providing or widening rear or side access lanes; [v] providing visitor parking within existing or proposed public streets. <p>1 Public open space</p> <p>PC12 Where appropriate, the development provides and improves land for public open space. New open space reserves should:</p> <ul style="list-style-type: none"> [i] be large enough and physically suitable for a local park or recreational facility; [ii] be substantially bounded by streets and fronted by dwellings; [iii] provide outlook for as many dwellings as practicable; [iv] protect any valuable natural features on a site; [v] be clearly accessible, useable and available 'to the broader community. <p>PC13 Where the development is next to existing public open space, it is to be designed to:</p> <ul style="list-style-type: none"> [i] complement the character and use of the adjacent open space; [ii] front the space by provision of a street along the boundary wherever practicable; [iii] enhance surveillance of urban open spaces; [iv] avoid 'claiming' or privatisation of the park by residents gardens or overflow uses from abutting dwellings. [vi] arranging the development to minimise outlook over side boundaries by ensuring that main upper-storey windows face the street, interior or back of the development; [vii] locating the multi-storey component of a development towards the street to limit impact on the amenity of adjacent private open space. <p>PC14 In larger scale new developments, varied dwelling types and forms are integrated along both sides of streets. This may be achieved by:</p> <ul style="list-style-type: none"> [i] providing a similar type or mix of buildings on both sides of a street to generate a consistent streetscape; [ii] ensuring dwelling fronts face dwelling fronts, not backs, across a street; [iii] not locating large-scale strongly contrasting dwelling types across the street from each other. Major changes in dwelling types should occur across rear site boundaries. <p>[b] Site features and constraints</p>	<p>be barely visible from the street.</p> <p>- No visitor car parking is proposed.</p> <p>- Private yards are provided for many of the units and the site has open space to the north and access to the small public park on Debbie Street.</p> <p>- Development will not result in detrimental impacts to adjoining open space to the north. This open space presently contains no significant improvements on the land.</p>
<p>2.2.2 Streetscape Character</p>	
<p>Performance Criteria</p> <p>[a] Built form</p> <p>PC1 In streetscapes of identified and valued character, the building frontage of an infill development is designed with reference to the major features that determine the character of the surrounding streetscape. Design elements to consider include:</p> <ul style="list-style-type: none"> [ii] massing and proportions; [iii] ground—floor height above ground level; [Hi] floor—to—ceiling height; [iv] roof form and pitch; [v] facade articulation and detailing; [vi] window and door proportions; [vii] features such as verandahs, towers, eaves, parapets; [viii] building materials, patterns, textures and colours; [ix] decorative elements; [x] kerb crossovers, fence styles and alignment. 	<p>Partially Complies - The front boundary to Harbour Drive is proposed to be screened by a 1.2m high plain timber panelled fence. The site plan indicates that a row of landscaping will sit behind this fence. This may potentially result in a fairly neutral frontage (though very simple) to the street. However, no details of the proposed landscaping have been provided.</p> <p>Landscaping is also pertinent to condition 15 which requires a landscaping buffer between the proposed units and neighbouring residential allotments on Harbour Drive and Debbie Street. Officers have requested Project Services implement significant landscaping to help ensure adequate screening and help ameliorate any visual impacts of the development from Harbour Drive.</p>

<p>PC2 In streets which do not have an identified or valued character, new development is designed to have an appropriate residential character, and detailed to enhance the streetscape.</p> <p>PC3 Street frontage setbacks are in accordance with Element 3 Building Envelope.</p> <p>PC4 To complement the scale of residential streetscapes, changes of building height between existing dwellings and adjacent walls of a new development are normally not more than one storey.</p> <p>PC5 Large walls which are highly visible from the street are adequately detailed for visual interest. This may be achieved, for example, by recesses, windows, projections, or variations of colour, texture or materials.</p> <p>b] Fences and street frontage</p> <p>PC6 Front fences enable outlook from dwellings to the street for safety and surveillance. As a guide, they should be a maximum of 1.2 metres high if solid, or 1.5 metres high if more than 50% transparent.</p> <p>PC7 Solid front fences to 1.8 metres high are to be minimised. Where used, they are limited to part of the frontage, and used only where appropriate to the streetscape character; where the principal private open space is in front of and to the north of the dwelling; and some surveillance of the street from dwelling windows is maintained.</p> <p>PC8 Garages and parking structures are sited and detailed to ensure they do not dominate the street frontage.</p> <p>PC9 Facilities in the frontage area such as gates, letter boxes, garbage bin enclosures, housing for meters and service connections and kerb crossovers are compatible in design with the fences and character of the development.</p>	<p>- The height of the buildings involved is consistent with the generally two storey single detached development that characterises the Bluewater Estate.</p> <p>- Front fences are 1.2m in height though it is unknown what view shed of the street will be provided given the absence of details regarding landscaping.</p> <p>- Parking areas are internal to the site and a single driveway access should not dominate the street front.</p> <p>- Unknown – Details on proposed services have not been supplied.</p>
<p>2.2.3 Building Envelope Element</p>	
<p>[a] Builtform</p> <p>PC1 The setback of dwellings from the street frontage is appropriate to the efficient use of the site, the comfort of residents and the streetscape.</p> <p>PC2 Dwelling and outbuilding walls are constructed in locations and of a length and height to ensure that there is no significant loss of amenity to adjacent residents.</p> <p>PC3 The building form and site layout are designed to limit the amount of impervious surfaces and to maximise on-site stormwater infiltration or detention.</p> <p>[b] Daylight and sunlight</p> <p>PC4 Habitable rooms within the development receive adequate daylight for carrying out daily tasks.</p> <p>[c] Breezes</p> <p>PC5 The building form and site layout are designed to maximise breezes while not inhibiting breeze circulation to adjoining properties.</p>	<p>Complies – The buildings on the site achieve the setback requirements specified by the Scheme, with building set back 6m from the front boundary and 3m from the side boundaries. Impacts on privacy and amenity are further mitigated by the unit closest to neighbouring residential allotments limited to a single storey.</p> <p>The height of the buildings is also fully compliant with the provisions of the Scheme (limit 11.5m). This 11.5m height limit would potentially allow up to four storeys on the site. As discussed above, the orientation of the dwelling units on the site should help to achieve satisfactory outcomes for passive surveillance, with the exception of the path to Wendy Street.</p>

2.2.7 Car Parking & Vehicle Access Element	
<p>PCI Resident and visitor parking is provided according to projected user needs, and takes into account: the number, size and type of dwellings proposed; [ii] the availability of public transport; the availability of on-street or nearby parking; [iv] local traffic and parking management plans and safety considerations; [v] the reduction of on-street spaces associated with provision of off-street spaces; [vi] the availability of land where existing buildings are being converted to residential use.</p> <p>PC2 Car parking facilities are designed and located: [1] reasonably close and convenient to dwellings; [ii] to be secure or allow surveillance from dwellings; [iii] to be lit at night; [iv] to be well-ventilated if enclosed; [v] to not obscure the view between the street and front windows of dwellings; [vi] to clearly define any visitor parking; [vii] to be separated from habitable room windows to minimise noise and fumes entering dwellings.</p> <p>PC3 Car parks, accessways, driveways and streets allow comfortable, safe and efficient vehicle movement and good connections to the existing street network, while minimising loss of on-street parking.</p> <p>PC4 Developments abutting arterial roads minimise the number of access points, provide safe access, and ensure vehicles egress in a forwards direction.</p> <p>PC5 Surface parking areas and accessways are designed, surfaced and graded to facilitate stormwater infiltration onsite, and landscaped with shade trees and screening vegetation where practicable.</p> <p>PC6 The layout and site management will enable appropriate use of any car spaces not specifically allocated to a dwelling.</p> <p>PC7 Car parking facilities, including accessways, carports and garages do not dominate the view of the development from the street, and are visually compatible with the development.</p> <p>[a] Number of spaces In accordance with Table D1 Carparking Requirements for Multiple Dwellings.</p>	<p>Does Not Comply – The car parking situation represents the primary non-compliance issue with the development. Nineteen (19) covered spaces are proposed, of which four (4) are reserved as disabled spaces. The Scheme identifies the requirement of twenty-nine (29) spaces – being one covered space for every unit; plus 1 uncovered space per every two units; and the provision of a wash down bay.</p> <p>As detailed in the submission to Project Services, any parking issues on the site are likely to be compounded by the nature of Harbour Drive, being very narrow road width from the median strip, street plantings within the road reserve and the proximity to the roundabout.</p> <p>- The road reserve of Harbour Drive does not permit any on street car parking due to the factors above.</p> <p>- Only one access point is proposed.</p> <p>- Does Not Comply – see above.</p>
2.2.8 Visual & Acoustic Privacy & 2.2.9 Landscaping	
<p>PCI The private open spaces and living rooms of adjacent dwellings are protected from direct overlooking by dwelling layout, screening devices, distance or landscaping.</p> <p>PC2 Windows of one dwelling are not located opposite the window of another dwelling unless direct views are restricted or windows are offset or sufficiently distant. [bi Acoustic privacy</p> <p>PC3 The transmission of noise between dwellings is minimised. This may be achieved by: [i] living rooms or garages of one dwelling not abutting bedrooms of adjacent dwellings; [ii] plumbing for each dwelling being separated and contained sufficiently to prevent the transmission of noise between dwellings. [iii] use of appropriate noise resistant wall, ceiling and floor materials.</p> <p>PC4 Active recreation facilities including swimming pools, spas, tennis courts and barbecue areas and services such as garbage chutes, pumps,</p>	<p>Unknown – As no details of the nature of this landscaping have been provided it is unknown as to whether the development can appropriately achieve the required outcomes.</p> <p>- Complies – overlooking issues between individual dwelling units should be minimal given the orientation of buildings.</p> <p>- No details on materials have been provided</p>

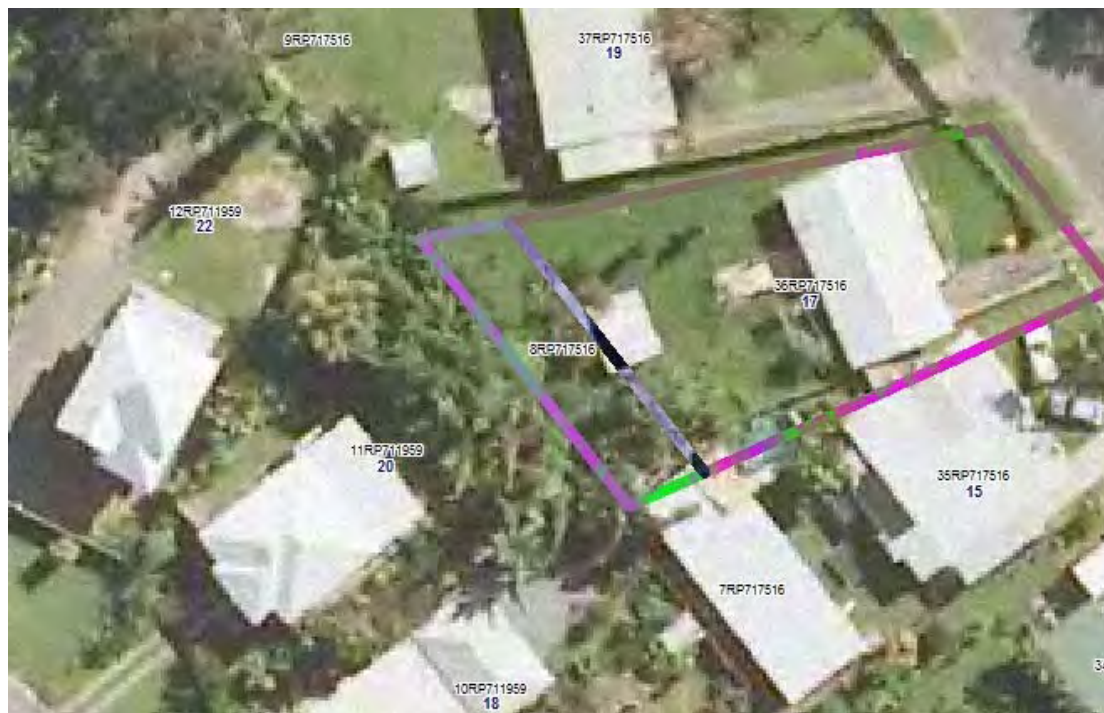
<p>compressors and other plant which generate high noise levels are located away from bedrooms of adjacent dwellings.</p> <p>Landscaping</p> <p>PC1 Open space around dwellings is allocated to individual units as far as practicable to facilitate management, and to minimise communal space and associated body corporate maintenance costs.</p> <p>PC2 Unpaved or unsealed landscaped areas are maximised and are designed to facilitate onsite infiltration of stormwater runoff.</p> <p>PC3 Major existing trees are retained wherever practicable, and buildings are located and designed to protect both buildings and trees from damage.</p> <p>PC4 The landscape design specifies the location and species of trees, shrubs and ground cover in a way that:</p> <p>[1] uses vegetation types and landscaping styles that blend the development in with the streetscape;</p> <p>[ii] should not affect the structure of the proposed buildings;</p> <p>[iii] contributes appropriate planting to streets fronted by the development;</p> <p>[iv] considers personal safety, by ensuring good visibility along streets, paths and driveways and avoiding shrubby landscaping near dwelling entries;</p> <p>[v] contributes to energy efficiency and amenity by providing substantial shade in summer especially to west-facing windows and open car park areas;</p> <p>[vi] improves privacy between dwellings;</p> <p>[vii] minimise risk of damage to overhead and underground power lines and other services;</p> <p>[viii] considers sightlines for vehicles and pedestrians, especially near street corners and intersections.</p>	<p>- Private yards are provided and communal space is limited to parking areas and the rear pedestrian access pathway.</p> <p>- The majority of open space on the site is indicated as turf. Impervious surfaces are limited to buildings, driveways and parking areas.</p> <p>-No significant trees are contained on site.</p> <p>- Unknown</p>
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Comments

The limited information, particularly with respect to landscaping makes it difficult to make a full assessment of the development against the relevant provisions of the Balance Scheme for the City of Cairns. The proposal would appear to be substantially consistent with the type and scale of development intended by the Special Facilities Zone, with the most notable exception being the provision of parking.

Earlville

The proposed Public Housing development is located at 17 O'Hara Street, Earlville, on land described as Lots 8 and 36 on RP717516.



The proposal involves the construction of ten (10) one-bedroom units within a three storey building. The site has a current development permit for multi-unit housing (8/7/). The approval permits the development of nine (9) two-bedroom units within a four storey building. The application was assessed against the requirements of CairnsPlan 2005 and was determined under Instrument of Delegation on 26 October 2007. The approval will lapse on the 26 October 2011.

The proposed public housing project is of a smaller scale to the approved development. A copy of the submitted plans and letter are contained in Appendix 3. An assessment of the project against the relevant codes under CairnsPlan 2009 is provided below:

Residential 3 Planning Area: Code Assessable

Residential 3 Planning Area Code

Residential Density

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P1 Site population density must be compatible with the desired development outcomes of this Planning Area.	A1.3 The site population density is not greater than 300 persons per hectare	Complies – Site population density is approximately 123 persons per hectare.

Built Form

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P2The height of all buildings must be compatible with the desired character and amenity of medium density residential areas and the height of buildings must not adversely affect visual amenity.	A2.1 Buildings are not more than 11.5 metres in height; or A2.2 Buildings are not more than 7.5 metres in height in the Gordonvale – Goldsborough District where the site is affected by the Cultural Heritage Area Overlay.	Complies - The building is approximately 8.6 meters in height

P3 Buildings must be setback to: a) maintain the character of residential neighbourhoods; and b) achieve separation from neighbouring buildings and from road frontages.	A3.1 No acceptable measures are specified. Note: Except for Multiple Dwellings, Retirement Villages and Dual Occupancies, the design and siting of buildings shall satisfy the provisions of the relevant Land Use Code.	See comments below.
P4 The site coverage of all buildings must not result in a built form that is bulky or visually obtrusive.	A4.1 No acceptable measures are specified.	Complies – Site cover is 36.67%

Character and Community Design

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P5 Development does not adversely affect the amenity of the: a) planning area; and b) adjoining land uses.	A5.1 No acceptable measures are specified.	Complies – Multi unit housing is consistent with the intent of the Residential 3 Planning Area.
P6 Buildings, structures and associated services are responsive to the natural features and constraints of the land.	A6.1 The design of buildings, structures and associated services takes into account: a) established trees; b) significant vegetation; c) ecological values; d) slope; e) waterways; and A6.2 On land steeper than 25% (1 in 4) alternative construction methods to slab on ground are utilised.	Complies
P7 Buildings and structures are: a) responsive to the tropical climate by taking into account prevailing breezes and solar orientation; and b) designed to minimise energy consumption.	A7.1 Buildings and structures are sited having regard to: a) significant views and vistas; b) predominant breezes; c) slope; d) solar orientation.	Complies
P8 Provision of physical and social infrastructure in developing residential neighbourhoods are facilitated through the orderly and sequential development of land.	A8.1 Development adjoins existing or approved development.	Complies

Multiple Dwelling and Holiday Accommodation Code

Site Requirements

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P1 The premises has sufficient area and dimensions to accommodate: a) the buildings; and b) other structures; and c) open space; and d) car parking; and e) vehicular access; and f) pedestrian access; and g) landscaping; and h) recreational facilities.	A1.1 Tourist and Residential Planning Area : Minimum Area = 600m ² . A1.2 The site has a minimum Main Road frontage of 15 metres	Complies – Site is 851m ² .

Built Form

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE										
<p>P2 To retain the existing character of the residential suburbs of Cairns, buildings are designed to:</p> <ol style="list-style-type: none"> maximise the separation between buildings; and provide adequate landscaping and open space around buildings for screening and recreational opportunities for the residents and visitors of the site; and achieve a balance between parking, driveways, service areas and landscaping and building; and promote through breeze, natural light, vistas and visual lines; and reduce the bulk of buildings. 	<p>A2.1 The maximum site cover is 40%; and</p> <p>A1.4 The gross floor area shall not exceed the following:</p> <table border="1"> <thead> <tr> <th>Planning Area</th> <th>Maximum GFA</th> </tr> </thead> <tbody> <tr> <td>Residential 2</td> <td>0.8 x site area</td> </tr> <tr> <td>Residential 3</td> <td>1.2 x site area</td> </tr> <tr> <td>Tourist & Residential</td> <td>1.2 x site area</td> </tr> <tr> <td>Other Areas</td> <td>No acceptable measure specified</td> </tr> </tbody> </table>	Planning Area	Maximum GFA	Residential 2	0.8 x site area	Residential 3	1.2 x site area	Tourist & Residential	1.2 x site area	Other Areas	No acceptable measure specified	<p>Complies – Though no accurate numbers for GFA are supplied the development is capable of satisfying the Performance Criteria.</p>
Planning Area	Maximum GFA											
Residential 2	0.8 x site area											
Residential 3	1.2 x site area											
Tourist & Residential	1.2 x site area											
Other Areas	No acceptable measure specified											
<p>P3 The development achieves tropical character and a Cairns vernacular through:</p> <ol style="list-style-type: none"> the use of structural elements and building materials of varying scales and textures; and variations in exterior colours; and variations in the size and patterning of windows; and use of awnings and other sun protection devices. 	<p>A3.1 No acceptable measures are specified.</p> <p><i>See Figure 1 below for an example</i></p>	<p>Appears to Comply –no details regarding colours or finishes have been provided.</p>										
<p>P4 The building is designed and sited to ensure adequate natural daylight in habitable rooms and principal open space areas, to allow residents and visitors enjoyment during the majority of the daylight hours.</p>	<p>P4.1 No acceptable measures are specified</p>	<p>Complies</p>										

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P5 The building bulk is reduced by a combination of:</p> <ol style="list-style-type: none"> balconies; and recesses in the vertical plane; and variation in building form; and roof features; and the use of curves, steps, recesses, projections or variation in plan and elevation, including articulated or separated balconies. 	<p>A5.1 Building bulk and form is reduced by:</p> <ol style="list-style-type: none"> the length of elevation does not exceed 15 metres, with a minimum change in plane in plan or elevation of 1.5 metres; and there is a minimum distance of 7 metres between adjoining residential buildings within the site; and the use of curves, steps, recesses, projections or variations in plan and elevation; and 	<p>Complies</p>

	<p>d) the provision of articulated or separated balconies; and</p> <p>e) variation in building colours, materials and textures; and</p> <p>A5.2 Rooflines are broken up and include features that reduce the overall bulk and add scale (such as cascading roof levels, gables, skillions or variations in pitch); and</p> <p>A5.3 The length of any continuous eaves line does not exceed 16m.</p>	
<p>P6 The overall scale, proportions and rhythm of the development:</p> <p>a) responds to traditional subdivision patterns; and</p> <p>b) is sympathetic to the traditional streetscape pattern.</p>	<p>A6.1 No acceptable measures are specified.</p> <p><i>See Figure 2 below for an example</i></p>	<p>Complies – the O'Hara Street elevation incorporates balconies for the front units. The bulk of the vertical façade broken up by varying roof structures and wall planes.</p>

Figure 1 – Illustration of Desirable Character Attributes for Multiple Dwellings

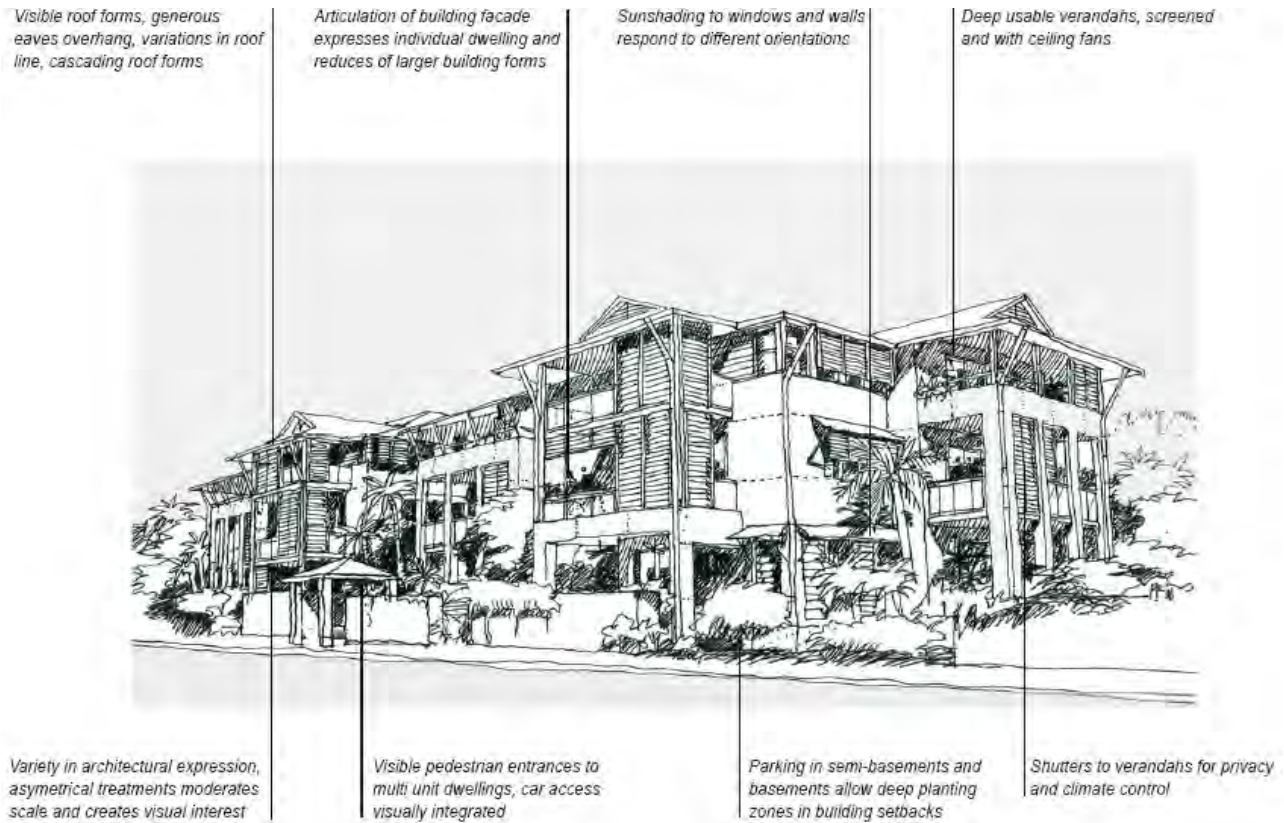
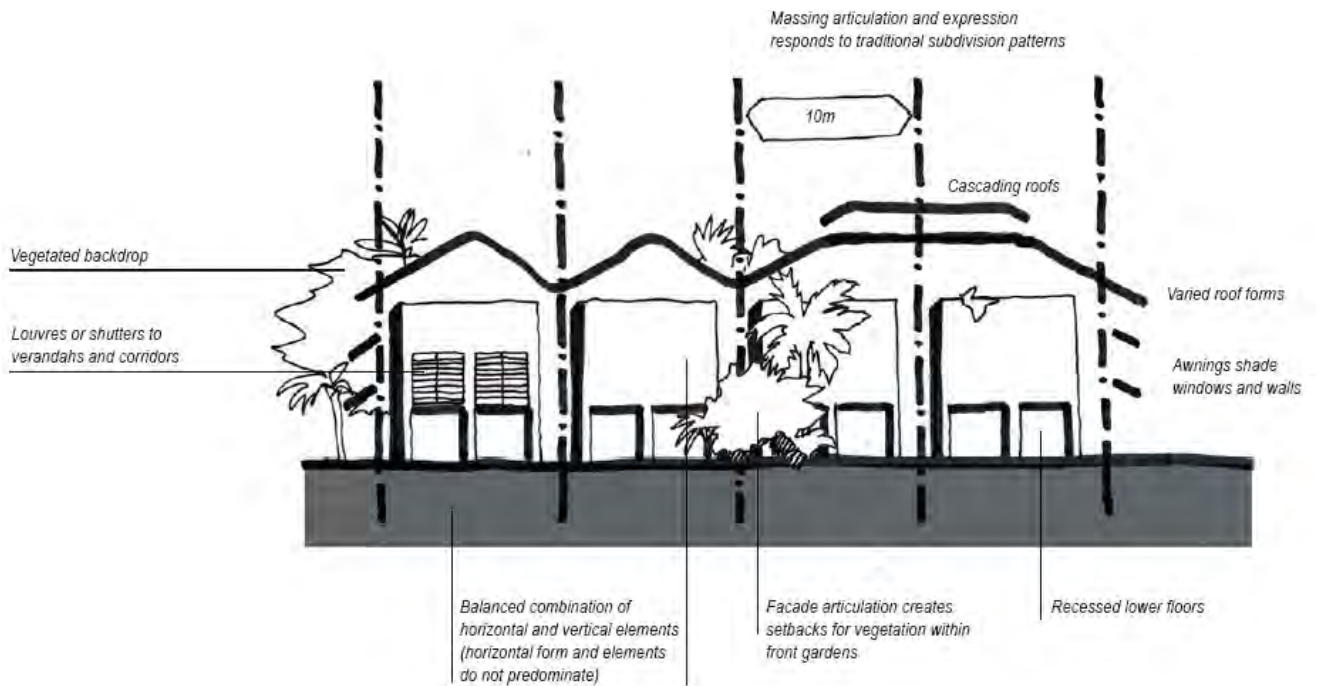

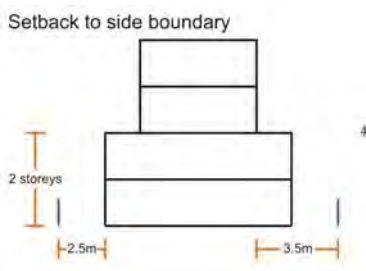


Figure 2 – Scale, Proportions and Rhythm of the Building Reflects the Dominant Streetscape

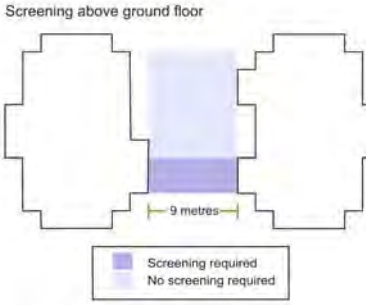


PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P7 Buildings shall be sited such that:</p> <ul style="list-style-type: none"> a) the setback from street frontages is consistent with the established streetscape pattern; and b) the setback from side and rear boundaries retains daylight access and privacy for adjoining properties; and c) the setback from all boundaries is sufficient to allow areas of deep planting; and d) the setback from street frontages provides for the desired streetscape pattern. 	<p>A7.1 Buildings are setback 15 metres to the Cairns Esplanade; and</p> <p>A7.2 For buildings in other locations, building setback from main road frontage is:</p> <ul style="list-style-type: none"> a) a minimum of 6 metres; and b) in established areas within 20% of the average setback of adjoining development; and <p>Figure 3 Example of front setback.</p>  <p>A7.3 Regardless of clauses 7.4 and 7.5 below, for the purposes of reducing overshadowing, the setback along the southern and western boundary shall be one third of the height of the building, measured to the eaves; and</p> <p>A7.4 The rear boundary setback is a minimum of 4 metres.</p> <p>A7.5 The side boundary setback is:</p> <ul style="list-style-type: none"> a) a minimum of 2.5m for the first 2 storeys; and b) a minimum of 3.5 m for the next 2 storeys, and <p>Figure 4 Example of Side Setback.</p> 	<p>The building has been setback 6 meters from O'Hara Street.</p> <p>The building is setback approximately 7m from Harpa Street and 5m from Oliva Street.</p> <p>The building is setback a minimum of 3.05 meters from the rear boundary.</p> <p>The building is setback a minimum of 2.5 meters.</p>

	Note: All setbacks are measured in plan view.	
P8 Services structures and mechanical plant (including air conditioners and split system air conditioners) are screened or otherwise incorporated as part of the building form.	<p>A8.1 Services structures and mechanical plant are not visible from:</p> <ul style="list-style-type: none"> a) the street, and b) adjoining properties, and c) public open space; and <p>A8.2 Mechanical plant is not located:</p> <ul style="list-style-type: none"> a) on balconies or adjacent to other liveable areas; and b) near multiple reflective surfaces such as walls and eaves; and <p>A8.3 Services and mechanical plant are located in building recesses or service areas such as parking and driveway areas.</p>	Unknown – It is not known whether air-conditioning is proposed. Normally a condition would be applied to the development for the provision of A/C screens.
P9 The development does not adversely impact on the natural environment and is designed to be sympathetic to natural constraints.	<p>A9.1 The siting and design of individual buildings within the development ensures:</p> <ul style="list-style-type: none"> a) the retention of existing mature vegetation on the site, with appropriate separation between vegetation canopy and root systems and buildings; or b) where selected removal of mature tree/s is required, suitable established tree/s are planted on site as a feature; and <p>A9.2 On land steeper than 25% (1 in 4) alternative construction methods to slab on ground are utilised.</p>	Unknown.

Amenity

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P10 Easily accessible and functional communal landscape and recreation area is provided for the benefit of residents.	<p>A10.1 A minimum of 35% of the site is communal landscaped and recreation area including:</p> <ul style="list-style-type: none"> a) at least 1/2 of the required landscaped area has a minimum dimension of 5m; b) deep planting to at least 1/3 of the required landscaped area, and c) paved and sealed areas do not exceed 1/3 of the required landscaped area; and <p>A10.2 For a development having 10 or more dwelling units at least 1 communal recreational feature is provided, including but not limited to a barbeque facility, swimming pool or children's play area.</p>	Unknown – No details on landscaping have been provided.

<p>P11 Easily accessible and functional private outdoor living area is provided for each unit</p>	<p>A11.1 Each dwelling unit, has a private and functional outdoor living area:</p> <ol style="list-style-type: none"> having minimum dimensions of 3 metres; and directly accessed from internal living spaces (not bedrooms). 	<p>Complies – Each unit has a small patio or verandah.</p>
<p>A12 The development is sited and designed to minimise overlooking of other dwelling units within the development, and of residential buildings on adjoining properties.</p>	<p>A12.1 Where habitable rooms look directly at habitable rooms in residential buildings within 2m at ground floor level or within 9m above ground floor level, privacy is protected by:</p> <ol style="list-style-type: none"> fixed obscure glazing in any part of the window below 1.5m above floor level; or fixed external screens; or sill heights of 1.5m above floor level; or in the case of screening for the ground floor level, fencing to a height of 1.8m above ground level. <p>Figure 5 Example of when Screening is Required</p> 	<p>Unknown.</p>
<p>P13 The development addresses the road frontage to facilitate casual surveillance and to enhance the amenity of the streetscape.</p>	<p>A13.1 The building has balconies, windows and verandahs that face the street; and</p> <p>A13.2 Visual privacy is provided to units that face the street through:</p> <ol style="list-style-type: none"> screening; and planting; and <p>A13.3 Fences and walls to road frontages are limited to:</p> <ol style="list-style-type: none"> 1.2 metres in height if solid; or 1.5 metres if partially transparent; or 1.8 metres if 50% permeable. <p>Note: Alternative solutions will be considered for developments on State Controlled Roads and future State Controlled Roads.</p>	<p>Units fronting O'Hara Street have verandahs. No details of screening or planting has been provided.</p> <p>No details on the fencing proposed for the site has been provided.</p>
<p>P13 A secure storage area is provided for each dwelling unit.</p>	<p>A13.1 Secure storage areas:</p> <ol style="list-style-type: none"> are located to enable access by motor vehicle; and have a minimum space of 3.5m² per dwelling unit; and have a minimum height of 2.1 metres; and are weather proof; and 	<p>Does Not Comply – No provision for storage appears to have been made from the plans supplied.</p>

	<ul style="list-style-type: none"> e) are lockable; and f) are ARI 100 immune. 	
<p>P14 Service structures and mechanical plant (including air conditioners and split system air conditioners) are located or insulated such that dwelling units within the development, and in residential buildings on adjoining properties are not adversely affected by the noise source.</p>	<p>A14.1 Service structures and mechanical plant are :</p> <ul style="list-style-type: none"> a) Screened and baffled; or b) Otherwise incorporated into the building form to maintain and uphold the amenity of dwelling units within the development, and of residential buildings on adjoining properties. 	Unknown.
<p>P15 Refuse storage areas are:</p> <ul style="list-style-type: none"> a) convenient and accessible to residents and refuse collection services; and b) located and designed to mitigate adverse impacts: <ul style="list-style-type: none"> i) within the site; and ii) on adjoining properties; and iii) to the street. 	<p>A15.1 Refuse storage areas:</p> <ul style="list-style-type: none"> a) are located on site; and b) are sited and designed to be unobtrusive and screened from view from the street frontage; and c) are imperviously sealed roofed and bunded, and contain a hose down area draining to Council's sewer network; and d) are of a sufficient size to accommodate bulk (skip) bins; and e) have appropriate access and sufficient on site manoeuvrability area for refuse collection services. 	Complies – Provision has been made for a refuse storage area.

Energy Efficiency

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P16 Buildings are sited and designed to:</p> <ul style="list-style-type: none"> a) maximise cross-breezes through the site; and b) minimise solar heat loads; and c) promote breeze and natural light. 	<p>A16.1 Buildings are oriented on site to:</p> <ul style="list-style-type: none"> a) allow prevailing south-east and north-east breezes to penetrate the site; and b) maximise the exposure of individual dwelling units to prevailing and cross-breezes; and c) minimise exposure to western sun; and <p>A16.2 Individual dwelling units are planned for maximum natural ventilation and light. Every dwelling unit has:</p> <ul style="list-style-type: none"> a) access to prevailing breezes including external walls with openings in at least two different orientations to allow breeze paths within the dwelling unit; or b) partitioned walls, vents in or above doorways and internal louvers as required to ensure air movement through the dwelling unit; and c) access to a covered, outside area accessible to breezes for the drying 	<p>Complies –</p> <p>Units provide for some cross ventilation.</p> <p>Units are not double banked.</p>

	<p>of clothes. This area shall be sited such that it is not visible from public spaces.</p> <p>A16.3 Units are not located on both sides of a central corridor (i.e. not double banked).</p>	
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Parking and Access

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
<p>P17 Vehicle access and parking for residents, visitors and service providers:</p> <p>a) does not dominate the street frontage; and</p> <p>b) does not impact on the road network; and</p> <p>c) is safe and convenient; and</p> <p>d) is accessible.</p>	<p>A17.1 The road providing access has a minimum road reserve width of 20 metres; and</p> <p>A17.1 The location of visitor parking is easily identified from the street; and</p> <p>A17.2 Where access is gated:</p> <p>a) there is a safe pullover area within the property boundary to use an intercom or gain access; and</p> <p>b) queuing space outside the gated for 2 vehicles using and awaiting the use of the intercom / access; and</p> <p>c) a minimum of two parking spaces are provided outside of the gated area and within the property boundary, and.</p> <p>A19.2 Vehicle parking is:</p> <p>a) illuminated at night; and</p> <p>b) close to the dwelling units to be served.</p>	<p>No visitor parking is provided.</p>
<p>P18 Residents Vehicle access and parking</p>	<p>A19.3 Access to basement parking is ARI 100 immune; and</p> <p>A19.4 Ventilation and pump-out infrastructure for car-parking areas is ARI 100 immune.</p>	<p>Does not comply - Ground floor parking does not appear to be Q100 flood immune.</p>
<p>P19 Pedestrian access is prominent and safe.</p>	<p>A18.1 A well-lit, sealed footpath, of minimum width 1.2m, links the development with the on street works; and</p> <p>A18.2 The development has at least one prominent, well-lit pedestrian entry with clear visibility for public surveillance.</p> <p>A18.3 Pedestrian access to the site is:</p> <p>a) via a well-lit and clearly delineated shared traffic area for developments of 4 dwelling units or less; and</p> <p>b) via a well-lit path that is separate from the vehicle access for all other developments; and</p> <p>A18.4 Buildings, fences and landscaping are designed and lit so that they will not conceal a person.</p>	<p>Complies – Provision has been made for the footpath to the front boundary, though it is not known as to what lighting/finishes is involved.</p> <p>A delineated pedestrian pathway on the driveway is proposed.</p>
<p>P20 The design and location of vehicle access and parking:</p>	<p>A20.1 A minimum of 1 metre wide dense planted buffer is</p>	<p>Complies – Undercover parking will be largely obscured from the street and</p>

<p>a) minimises impacts on adjoining dwellings and compliments the streetscape; and</p> <p>b) reduces thermal radiation, minimises noise and lessens the visual impact of hardstand area.</p>	<p>provided adjacent to any vehicle movement area.</p> <p>A20.2 Vehicle access and parking is:</p> <p>a) oriented or screened to minimise reflection of car headlights and area lighting reflecting into the windows of habitable rooms and patios or balconies of dwelling units and adjacent dwellings / properties; and</p> <p>b) separated from windows to habitable rooms either by location, enclosure or screening to minimise noise and light disturbance and fume emissions.</p> <p>A20.3 Vehicle parking structures are designed such that they:</p> <p>a) are compatible with overall building design in terms of roof form, detail, material and colours; and</p> <p>b) are consistent with stipulated setbacks for the buildings, and</p> <p>c) do not dominate the streetscape.</p>	<p>should result in minimal impacts to neighbouring developments.</p>
<p>P21 Bicycle parking for visitors and residents provides;</p> <p>a) secure parking facilities; and</p> <p>b) convenient access; and</p> <p>c) safe access; and</p> <p>d) protected from the sun and rain.</p>	<p>A21.1. On-site bicycle parking shall be provided at a rate of:</p> <p>a) 1 space per 16 dwelling units for visitor parking; and</p> <p>b) 1 space per 4 dwelling units for residential parking; and</p> <p>A21.2 Parking shall be designed and located to:</p> <p>a) enable wheels and frame to be locked to the device without damaging the bicycle; and</p> <p>b) be located outside pedestrian movement paths; and</p> <p>c) be easily accessible from the road; and</p> <p>d) be arranged so that parking manoeuvres will not damage adjacent bicycles; and</p> <p>e) be protected from manoeuvring motor vehicles and opening car doors; and</p> <p>f) be well lit; and</p> <p>g) be protected from the weather.</p>	<p>Unknown</p>
<p>P22 Visitor parking remains accessible and useable to visitors at all times.</p>	<p>A22.1 Visitor car parking bays are not allocated to individual dwelling units; and</p> <p>A22.2 Visitor car parking is not gated or located behind security doors/gates; and</p> <p>A22.3 Visitor car parking is visible from the street frontage, clearly signed and delineated;</p>	<p>Does Not Comply – No provision for visitor spaces has been made</p>

	<p>and</p> <p>A22.4 Visitor car parking bays are not provided in a tandem arrangement; and</p> <p>A22.5 Visitor car parking bays are not located in front of private storage rooms allocated to individual units; and</p> <p>A22.6 Visitor car parking bays are not provided in parallel formation along a driveway.</p>	
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Public Art

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	COMPLIANCE
P23 Public art is provided that is visible and accessible to the general public and reflects the local character of Cairns through a variety of mediums.	A23.1 Public art is provided in accordance with Planning Scheme Policy – Public Art.	N/A- contributions are not payable for public housing projects.

Vehicle Parking & Access Code

Vehicle Parking Numbers

PERFORMANCE CRITERIA	ACCEPTABLE MEASURES	
<p>P1 Sufficient parking spaces must be provided on the site to accommodate the amount and type of vehicle traffic generated by the development of the site, having particular regard to:</p> <ul style="list-style-type: none"> a) the desired character of the area in which the premises is located; and b) the nature and scale of the development; and c) accessibility to the premises; and d) the nature and frequency of public transport serving the area; and e) whether or not the development involves the retention of an existing building, particularly an identified historic building, and the previous requirements for car parking for the building; and f) whether or not the use involves the retention of other cultural heritage features or significant vegetation; and g) the different types of vehicles that visit the premises are adequately accommodated. 	<p>A1.1 The minimum number of parking spaces provided on the site is not less than the number prescribed in Schedule 1 to this Code for the particular development.</p>	<p>Does Not Comply – Schedule 1 prescribes that the development should make provision for 15 car parking spaces. The project currently proposes 7 spaces of which 1 is a disabled space.</p>

Comments

It is difficult to provide a full assessment of the impacts of the development on the neighbourhood given the very limited information provided. No assessment against the Landscaping Code is provided as no details on the landscaping were submitted. The scale of the development is generally consistent with CairnsPlan requirements. The main areas of non compliance relate to car parking and flood immunity.

A minimum of approximately one (1) meter of fill will be required to achieve flood immunity for the ground floor of the development. Careful consideration should be given to the design to ensure that the flow of all external storm water from the property is directed to a lawful point of discharge such that it does not adversely affect surrounding properties.

APPENDIX 1 – NOTIFICATION & PLANS FOR PALM COVE DEVELOPMENT



Project Services
Queensland Government

24 November 2009

Chief Executive Officer
Cairns Regional Council
Planning and Development Services
PO Box 359
Cairns Qld 4870



Department of
Public Works

Attn: Planning & Development Manager

Dear Sir/Madam,

**PROPOSED PUBLIC HOUSING DEVELOPMENT
15 HARPA STREET, PALM COVE**

Project Services, a Business Unit within the Department of Public Works, has been appointed to act on behalf of the Department of Communities in relation to the carrying out of public housing developments in accordance with Chapter 5 Part 6 of the *Integrated Planning Act 1997* (IPA).

It is the Department's intention to develop the above site, described as Lot 65 on RP868442 for the purpose of developing Eighteen, two-bedroom apartments.

The project is being undertaken under the Nation Building Economic Stimulus Plan (NBESP). The Federal Government is providing some \$1.2 billion towards the NBESP to be completed or committed by June 2011. The State Government has undertaken to deliver a program of 4,000 units of social housing under this Plan.

I advise that the Director General, Department of Communities has determined that, in accordance with Chapter 5 Part 6.4.1 of the *Integrated Planning Act 1997*, the project at 15 Harpa Street, Palm Cove is 'not substantially inconsistent' with the relevant Planning Scheme and will progress to construction. In accordance with the requirements of IPA Chapter 5 Part 6.5.5, this letter and the enclosed drawings provide information regarding the proposed public housing development.

Should you have any further queries relating to this matter please do not hesitate to contact this office on 3227 8750.

Yours sincerely,

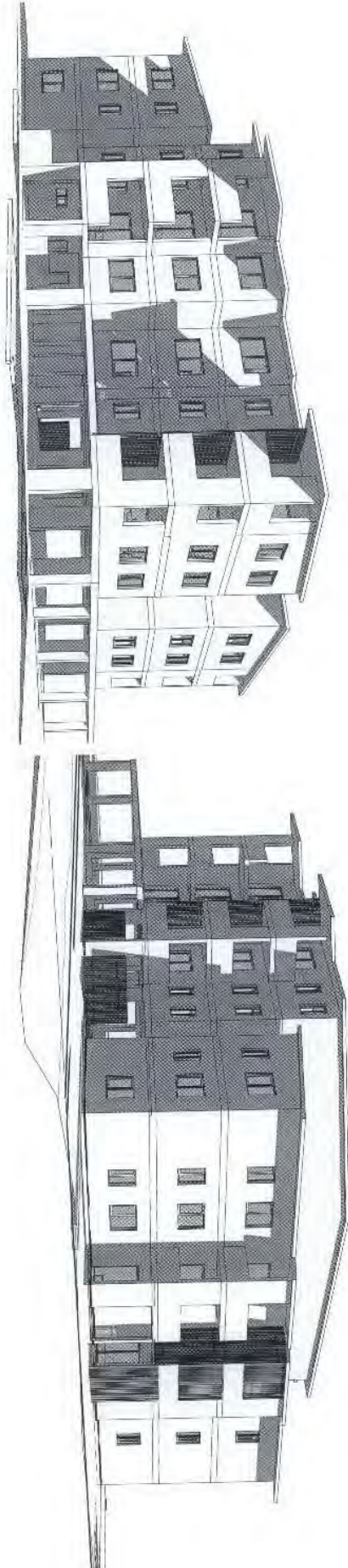
Kelly Graham
Town Planner
Housing & Property Portfolio
Project Services

Brisbane Office
Level 4 80 George Street Brisbane
GPO Box 2906 Brisbane
Queensland 4001

Telephone 07 3227 8790
Facsimile 07 3227 8790
Kelly.graham@projectservices.qld.gov.au

Assured to AS/NZS ISO 9001:1994
Environmental Management to
AS/NZS ISO14001:1996

UNIT DEVELOPMENT 'ROCKWOOD'



SKETCH ISSUE ONLY - NOT FOR CONSTRUCTION

ARCHITECTURAL DRAWINGS

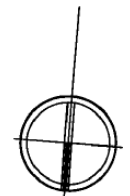
14 OLIVA ST PALM COVE CAIRNS

OWNER : CLIENT
 PH: _____
 M: _____
 F: _____
 ADDRESS : _____
 BILLING ADDRESS _____

DESIGNER :
 MERCURY DESIGN
 PH: 1 300 133 917
 M: 1 300 133 918
 ADDRESS : _____
 P.O. BOX 227
 BULLHORN QLD 4171

ENGINEER :
 ADDRESS : _____

REVISION	DATE
CONCEPT	27-10-09



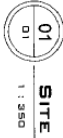
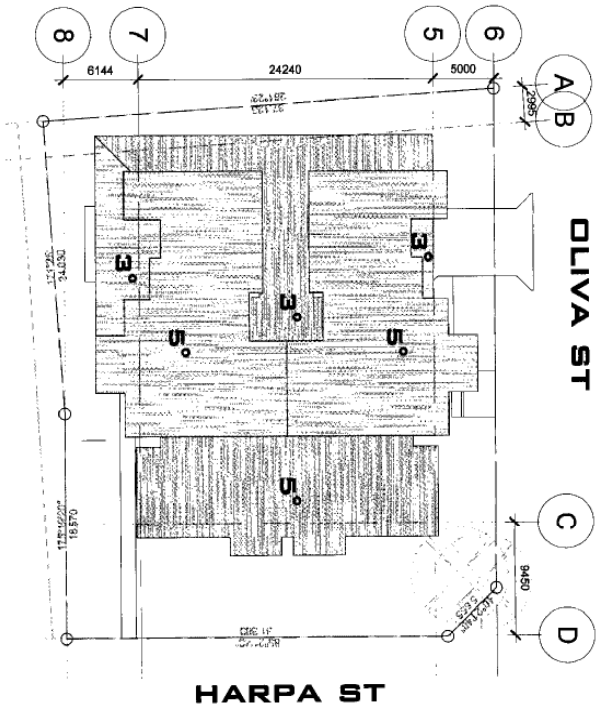
SITE INFORMATION:

R.P.D.:
 LOT NO.: 65
 PLAN NO.: RP 668442
 PARCEL: 5000 SQ. METERS
 ZONING: R150
 AREA: 1565M²

STORMWATER:
 ROOF WATER FROM HOUSE TO FLOW TO RAINWATER TANK AND THEN TO STREET TO JOIN EXISTING GROUND. SYSTEM VIA 90 DIA U.P.V.C. STORMWATER LINES AT 1:1.00 MINIMUM FALL.

RETAINING WALLS:
 IF REQUIRED ALL RETAINING WALLS SHALL BE PROVIDED IN STRICT ACCORDANCE WITH ENGINEERS DETAILED DESIGN AND LOCAL AUTHORITY REQUIREMENTS.

WIND CLASSIFICATION:
 SOIL CLASSIFICATION:
 REFER SOIL REPORT
 CLIMATE ZONE:
 REFER ENERGY EFFICIENCY REPORT



NOTE: SET OUT & HEIGHT IDENTIFICATION TO BE CARRIED OUT BY REGISTERED SURVEYOR PRIOR TO CONSTRUCTION

NOTE: DWELLINGS TO BE BUILT/ANEXABLY CONSTRUCTED

SKETCH ISSUE ONLY - NOT FOR CONSTRUCTION

ALL NEW WORK TO BE CARRIED OUT IN ACCORDANCE WITH WORKPLACE HEALTH & SAFETY ACT, REGULATIONS, ADVISORY STANDARDS AND WITH THE RELEVANT AUSTRALIAN STANDARDS AS A MINIMUM LEVEL OF AMENITY - THESE WILL INCLUDE BUT ARE NOT LIMITED TO:

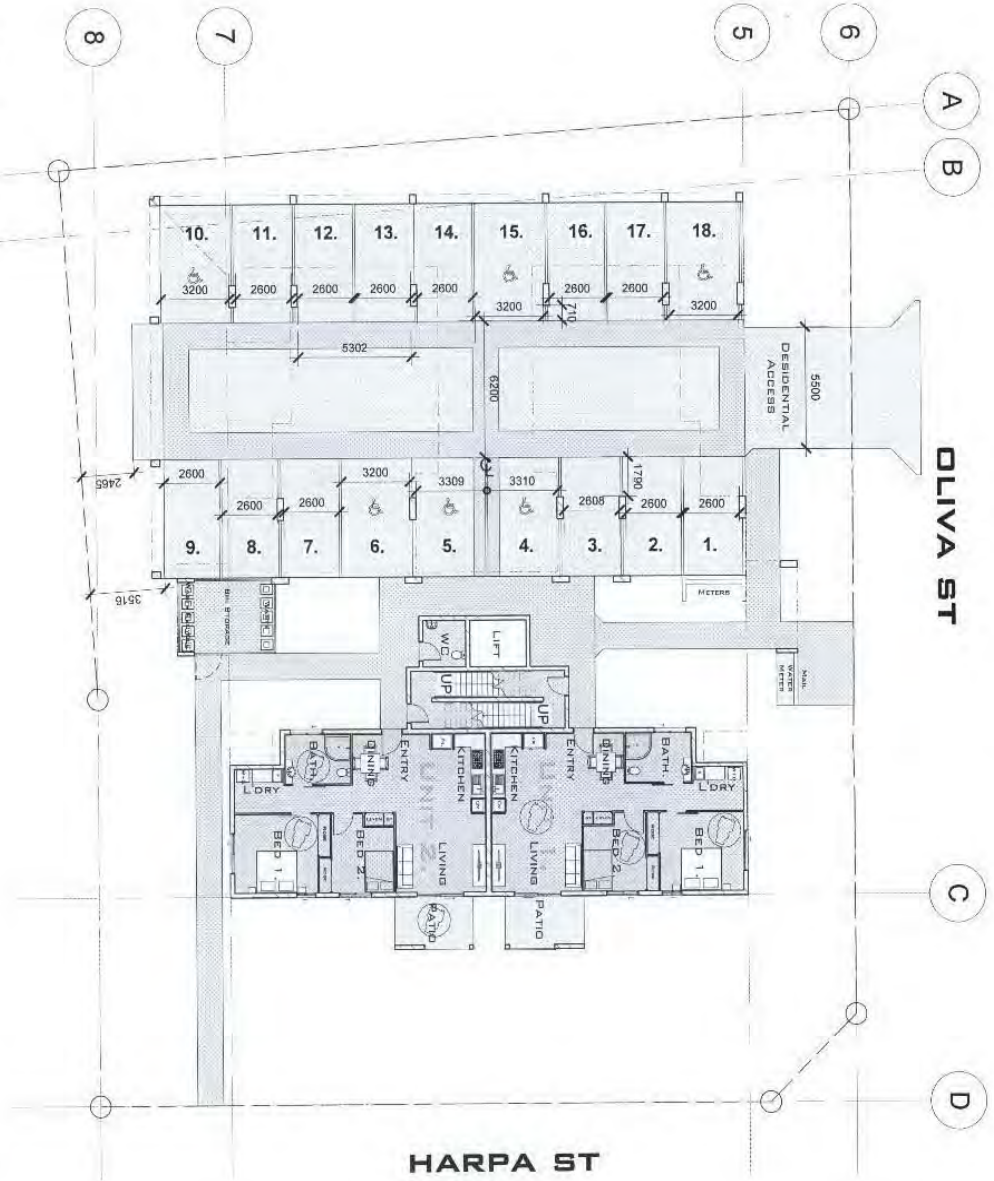
PLUMBING - AS3500 (2003)
 WATER PROOFING - AS3740 (2004)
 ELECTRICAL - AS3000 (2000) 1720 (1990) & AS 4440 (2004)
 TIMBER - AS 1684 (1999) AS 3660 (2000)
 STEEL STRUCTURES - AS 4100 (1996) & AS 4600 (1996)
 CONCRETE & MASONRY - AS 3600 (2001) & AS3700 (2001)
 DEMOLITION - AS 2301 (2001) & AS 2370 (1996)
 STRUCTURAL DESIGN - AS 1170 (2002)
 AS 1170 (2002) AS 1726 (2003)
 ACCESS & EGRESS - AS 2947 (1999) & AS 1298 (1994)
 AIR CONDITIONING - AS 1688 (2001)
 FIRE DETECTION - AS 1670 (2004)
 ROOF SAFETY - AS 4894 (2004)
 PARKING - AS 2898 (2004) AS 4248 (1998)
 BUILDING MANAGEMENT - AS 4913 (2000)
 QUALITY MANAGEMENT - AS 9001 (2000)

RETAIL & RESIDENTIAL & COMMERCIAL
 750 SM 257
 T: 1300 133 917
 F: 1300 133 916
 0/18 A WILSON RD 15372A
 MERCUY DESIGN
 MOORE PT., QLD A.S.N. 15372A

PROJECT:	UNIT DEVELOPMENT	EDITION:	CONCEPT	JOB NUMBER:	2009140
CLIENT:	ROCKWOOD	DATE:	27-10-09	SCALE:	1:380
ADDRESS:	14 OLIVA ST PALM COVE CAIRNS	CHECKED BY:	SH	PAGE:	01 OF 06

MERCURY DESIGN
 RETAIL + RESIDENTIAL + COMMERCIAL
 PO BOX 227 BULLMUDA BRISBANE AUSTRALIA 4171
 T: 1300 133 917
 F: 1300 133 916
 M: 08 4373 1207
 WWW.MERCURYDESIGN.COM
 FORMERLY LLOYD ARCHITECTURE

02
 1 : 200
GROUND FLOOR



PROJECT: UNIT DEVELOPMENT
 CLIENT: ROCKWOOD
 ADDRESS: 14 OLIVA ST PALM COVE CAIRNS

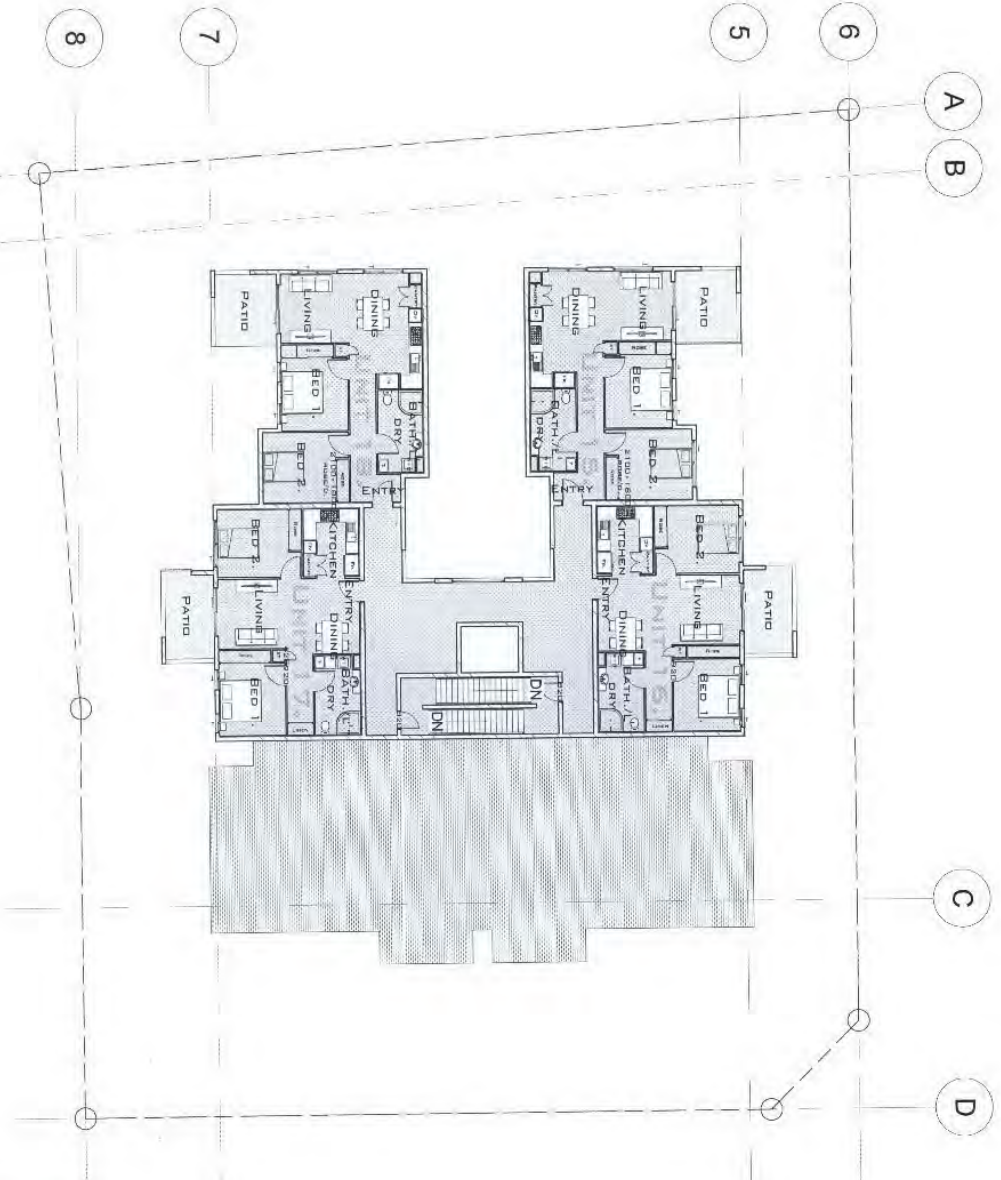
EDITION: CONCEPT Job Number: 2009140
 DATE: 27/10/09 SCALE: 1 : 200
 DRAWN BY: BI PAGE: 02 OF 06
 CHECKED BY:

SKETCH ISSUE ONLY - NOT FOR CONSTRUCTION

DISCLAIMER:
 This document is a preliminary sketch and is not intended for construction. It is provided for informational purposes only and should not be used for any legal or financial purposes. The design is subject to change without notice. The client is responsible for obtaining all necessary permits and approvals. The architect is not responsible for any errors or omissions in this document. This document is the property of Mercury Design and should not be reproduced or distributed without the written consent of Mercury Design.

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 RETAIL + RESIDENTIAL + COMMERCIAL
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 F: 1300 133 916
 G.P.B.N. LICENSE NO. 1287724
 HERB PTY LTD ABN 1204580798

1
 04
 1 : 200
THIRD FLOOR



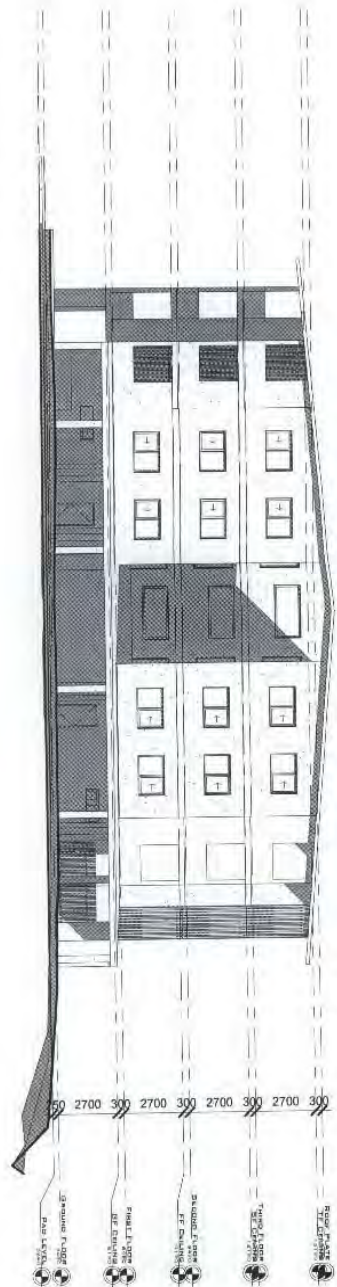
Project: **UNIT DEVELOPMENT**
 Client: **ROCKWOOD**
 Address: **14 OLIVA ST PALM COVE CAIRNS**

London: **CONCEPT** Job Number: **2009140**
 Date: **27-10-2009** Scale: **1 : 200**
 Drawn By: **BN** Paved: **04** of **06**
 Checked By:

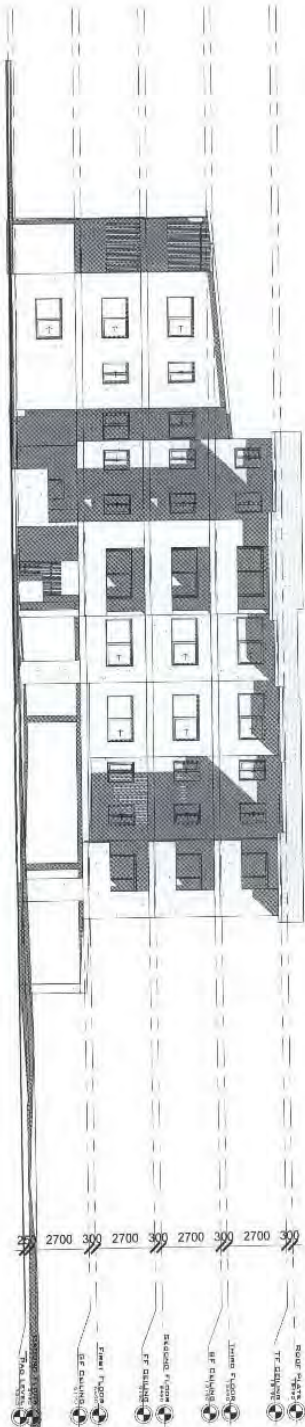
SKETCH ISSUE ONLY - NOT FOR CONSTRUCTION

SKETCH NOTES:
 All dimensions are to be confirmed by the architect prior to the building phase of the project.
 The floor plan is a sketch and is not intended for construction. It is for information only and should not be used for construction purposes.
 The floor plan is a sketch and is not intended for construction. It is for information only and should not be used for construction purposes.
 The floor plan is a sketch and is not intended for construction. It is for information only and should not be used for construction purposes.

04
03
1 : 200
PROPOSED ELEVATION SOUTH



05
05
1 : 200
PROPOSED ELEVATION WEST



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RETAIL, RESIDENTIAL, COMMERCIAL
PO BOX 257
T: 1 300 133 917
F: 1 300 133 916
D.B.A. LICENSE NO. 1029724
HEAD OFFICE: ARLIN 120550704

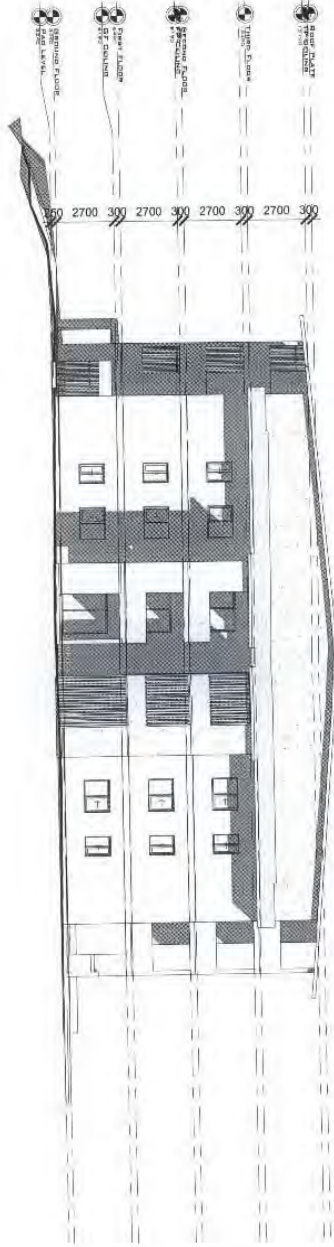
PROJECT: UNIT DEVELOPMENT
CLIENT: ROCKWOOD
ADDRESS: 14 OLIVA ST PALM COVE CAIRNS

EMPHASIS: CONCEPT JOB NUMBER: 2009140
DATE: 27/10/2009 SCALE: 1 : 200
DRAWN BY: SH PAGE: 05 OF 06
CHECKED BY:

06
D6

1 : 200

PROPOSED ELEVATION NORTH



07
D6

1 : 200

PROPOSED ELEVATION EAST



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 F: 1 300 133 915
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 1500 501 150 501 1500 501

PROJECT: UNIT DEVELOPMENT
 CLIENT: ROCKWOOD
 ADDRESS: 14 OLIVA ST PALM COVE CAIRNS

EDITION:	CONCEPT	JOB NUMBER:	2009140
DATE:	27-10-2009	SCALE:	1:200
DRAWN BY:	SH	CHECKED BY:	PAAR
			06 OF 06

APPENDIX 2 – NOTIFICATION & PLANS FOR TRINITY PARK DEVELOPMENT



3 March 2010
Ref: 56535

Chief Executive Officer
Cairns Regional Council
PO Box 359
Cairns QLD 4870



Department of
Public Works

Attention: Manager – Development Assessment

Dear Sir/Madam,

**PUBLIC HOUSING DEVELOPMENT
24 HARBOUR DRIVE, TRINITY PARK
NINETEEN, TWO BEDROOM APARTMENTS**

Project Services, a Business Unit within the Department of Public Works, has been appointed to act on behalf of the Department of Communities in relation to the carrying out of public housing developments in accordance with Chapter 9 Part 5 of the *Sustainable Planning Act 2009* (SPA).

It is the Department's intention to develop the above site, described as Lot 1801 on SP165903 for nineteen, two bedroom apartments.

The project is being undertaken under the Nation Building Economic Stimulus Plan (NBESP). The Commonwealth Government is providing some \$1.08 billion towards the NBESP to be completed or committed by December 2010. The State Government has undertaken to deliver a program of 4,000 units of social housing under this Plan.

I advise that the Director General, Department of Communities has determined that, in accordance with Chapter 9 Part.5 of the SPA, the project at 24 Harbour Drive, Trinity Park is 'not substantially inconsistent' with the relevant Planning Scheme and will progress to construction. In accordance with the requirements of SPA Chapter 5 Section 722, this letter provides information regarding the proposed public housing development.

Should you have any further queries relating to this matter please do not hesitate to contact this office on 3227 7088.

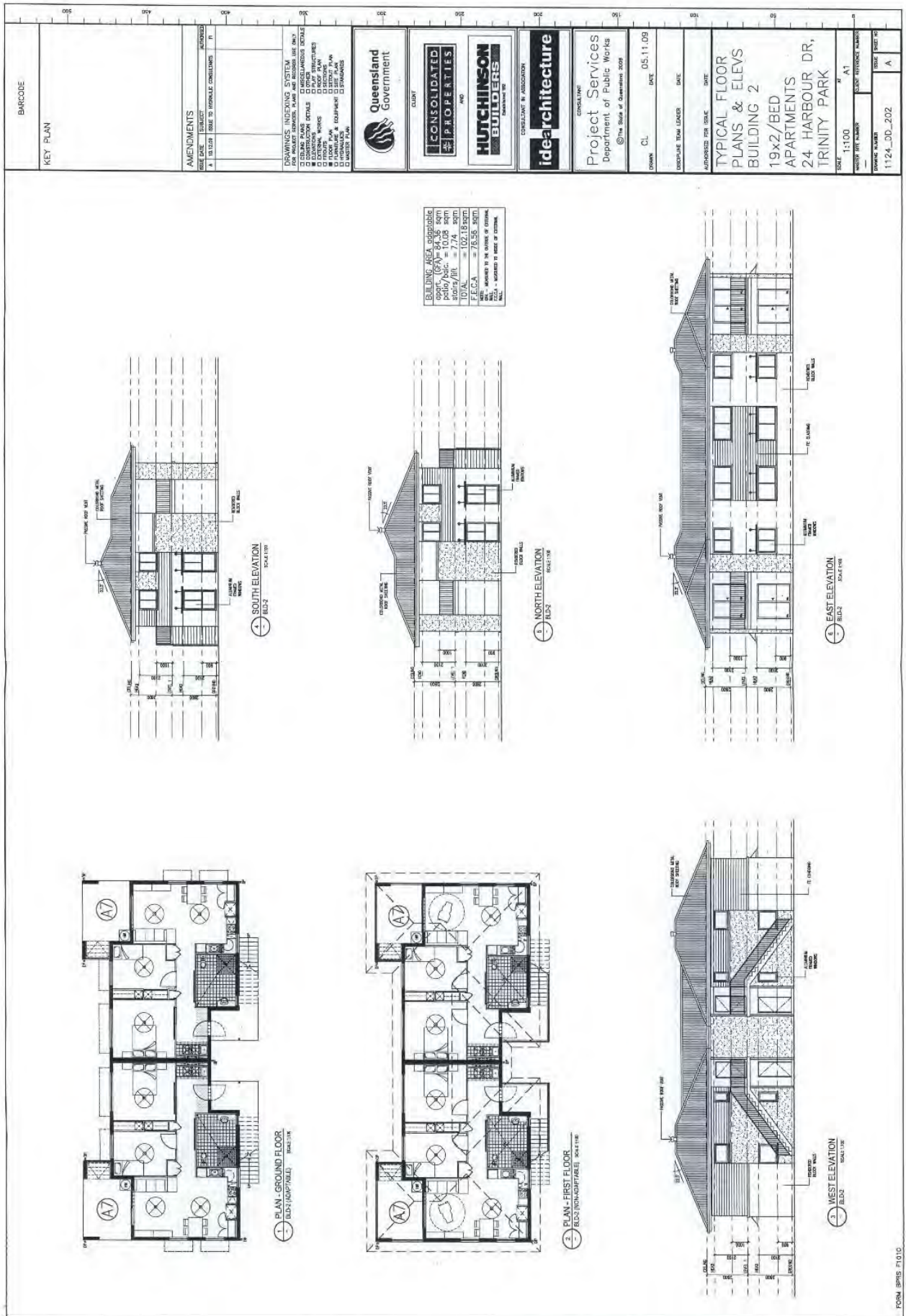
Yours sincerely,

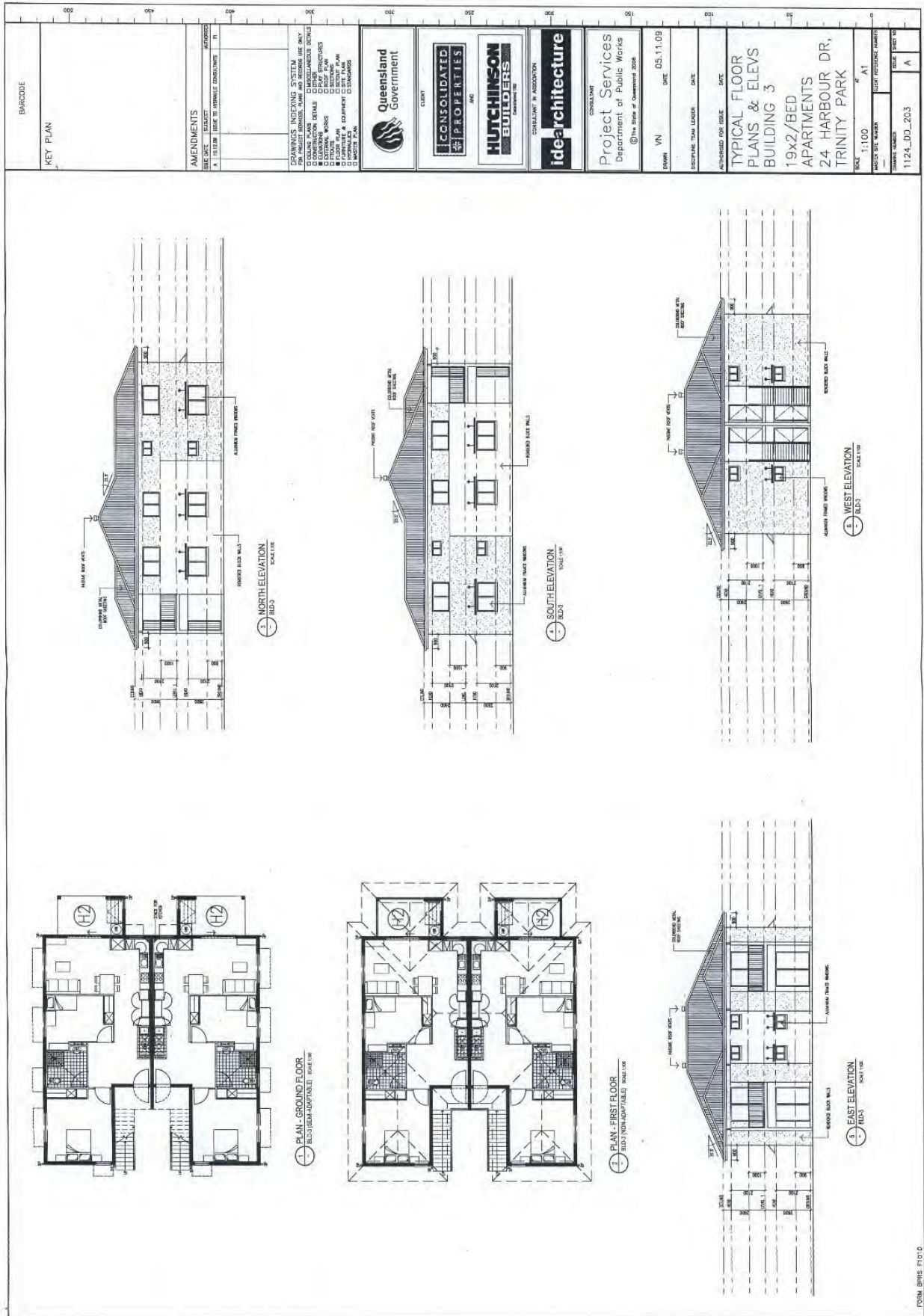
Chris Rivers
Town Planner
Housing & Property Portfolio
Project Services

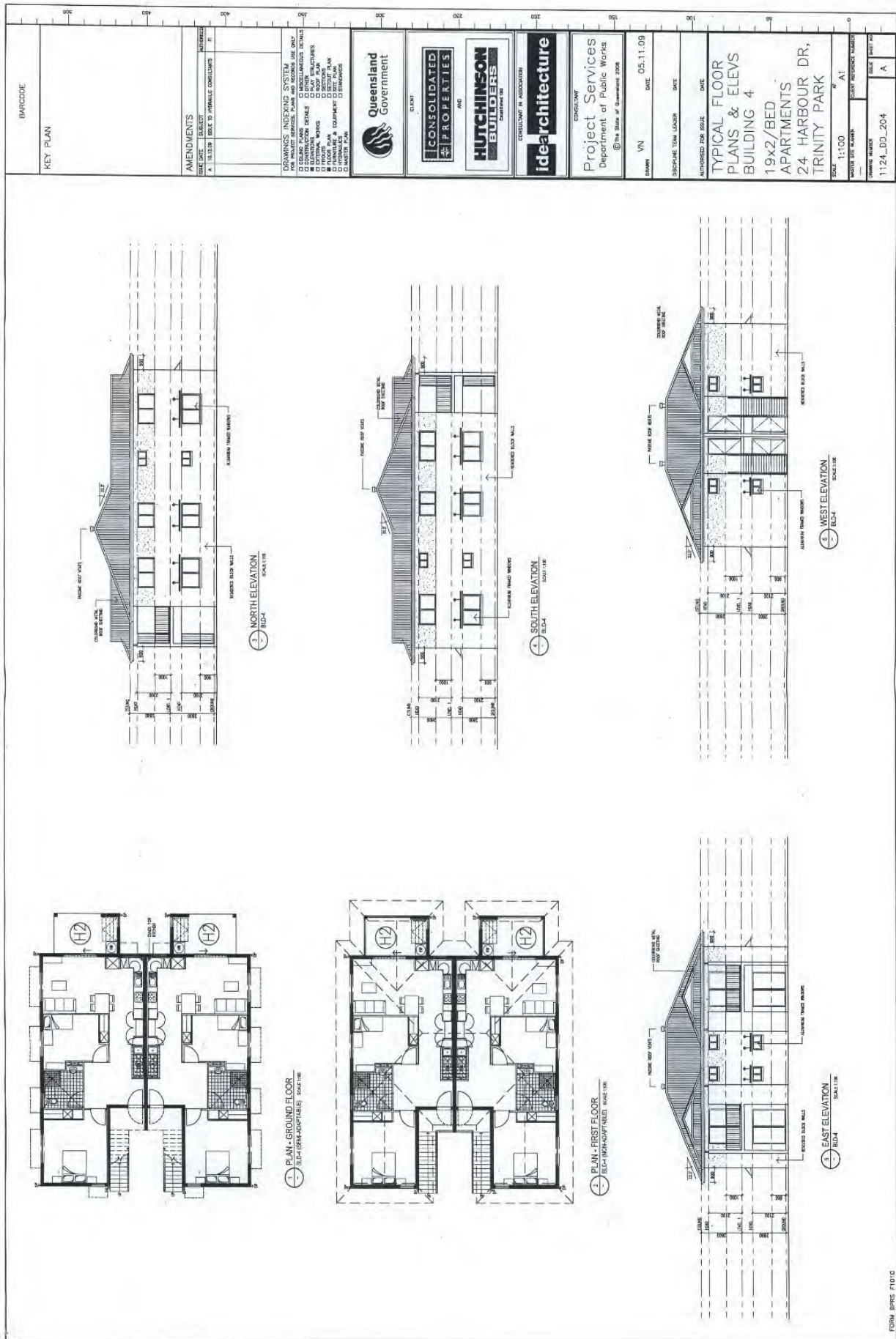
Brisbane Office
Level 4 80 George Street Brisbane
GPO Box 2908 Brisbane Queensland 4001

Telephone 3227 7088
Facsimile 3225 1204
Chris.rivers@projectservices.qld.gov.au

Assured to AS/NZS ISO 9001:1994
Environmental Management to
AS/NZS ISO14001:1996







APPENDIX – NOTIFICATION & PLANS FOR EALVILLE DEVELOPMENT



25 March 2010

Chief Executive Officer
Cairns Regional Council
Planning and Development
PO Box 359
Cairns Qld 4870

Attention: Planning Manager

Department of
Public Works



**PROPOSED PUBLIC HOUSING DEVELOPMENT
17 O'Hara Street, Earlvile**

Project Services, a Business Unit within the Department of Public Works, has been appointed to act on behalf of the Department of Communities in relation to the carrying out of public housing developments in accordance with Chapter 9, Part 5 of the *Sustainable Planning Act (SPA)*.

It is the Department's intention to develop the above site, described as Lots 8 and 36 on RP717516 for the purpose of developing ten, one-bedroom apartments.

The project is being undertaken under the Nation Building Economic Stimulus Plan (NBESP). The Federal Government is providing some \$1.2 billion towards the NBESP to be completed or committed by June 2011. The State Government has undertaken to deliver a program of 4,000 units of social housing under this Plan.

I advise that the Director General, Department of Communities has determined that, in accordance with Chapter 9 Part 5 of the *Sustainable Planning Act 2009*, the project at 17 O'Hara Street, Earlvile is 'not substantially inconsistent' with the relevant Planning Scheme and will progress to construction. In accordance with the requirements of SPA section 722, this letter and plans provides information regarding the proposed public housing development.

Should you have any further queries relating to this matter please do not hesitate to contact this office on 3227 8790

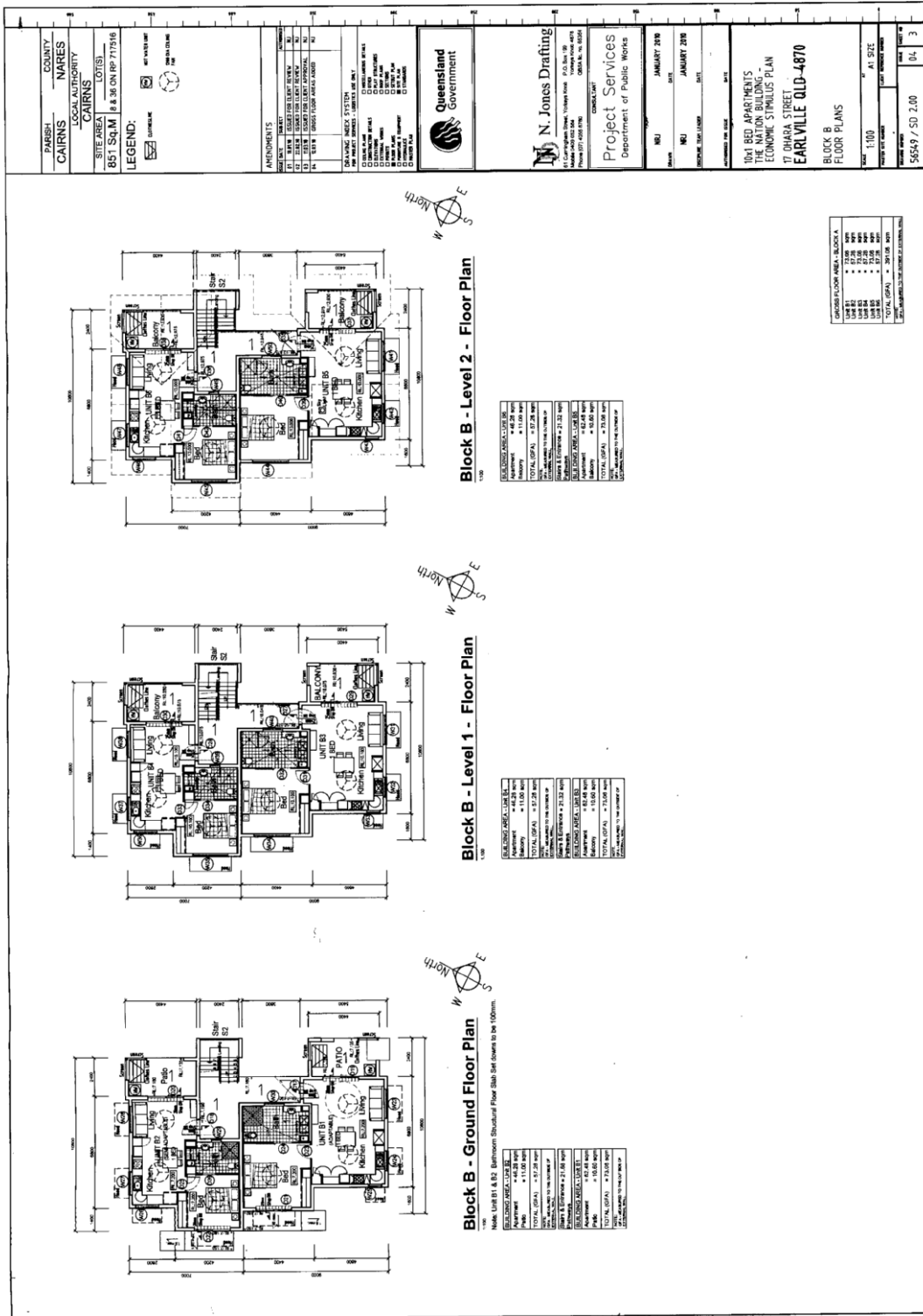
Yours sincerely,

Kelly Graham
Town Planner
Housing & Property Portfolio
Project Services

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GPO Box 2906 Brisbane
Queensland 4001

Telephone 07 3227 8790
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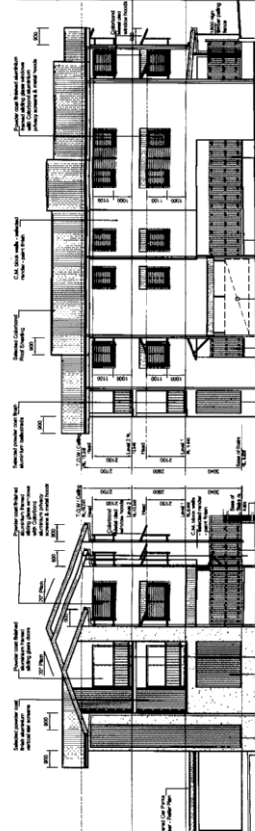
Assured to AS/NZS ISO 9001:1994
Environmental Management to
AS/NZS ISO 14001:1996



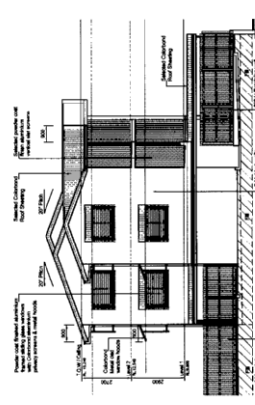
<p>PARISH: CAIRNS COUNTY: NARES LOCAL AUTHORITY: CAIRNS SITING AREA: 851 SQ.M. (8.36 ON RP 717616) LOT(S):</p> <p>LEGEND:</p>	<p>AMENDMENTS</p> <p>DATE: 11/11/09 NO: 1 DESCRIPTION: REVISION TO CLEARANCE HEIGHT</p> <p>DATE: 11/11/09 NO: 2 DESCRIPTION: REVISION TO CLEARANCE HEIGHT</p> <p>DATE: 11/11/09 NO: 3 DESCRIPTION: REVISION TO CLEARANCE HEIGHT</p>	<p>Queenland Government</p>	<p>N. Jones Drafting</p> <p>110 Campbell Street, Cairns QLD 4870 Ph: 07 4051 1111 Fax: 07 4051 1112 Email: n.jones@njonesdrafting.com.au</p>	<p>Project Services Department of Public Works</p> <p>DATE: JANUARY 2010 DRAWN BY: [Signature] CHECKED BY: [Signature]</p>	<p>1941 RED APARTMENTS THE NATION BUILDING - ECONOMIC STIMULUS PLAN 17 OHARA STREET EARLVILLE QLD 4870</p> <p>ELEVATIONS</p> <p>SCALE: 1:100 SHEET NO: 03 OF: 4</p>
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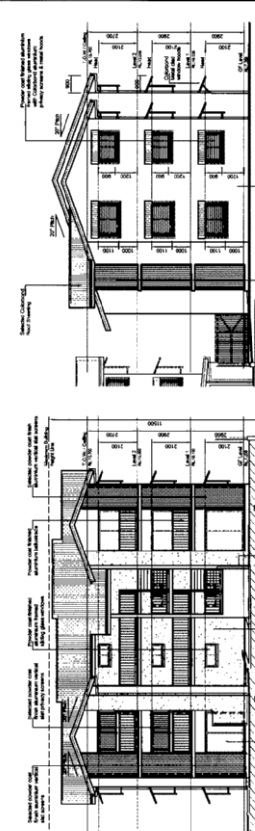
Elevation - Southern - Block A



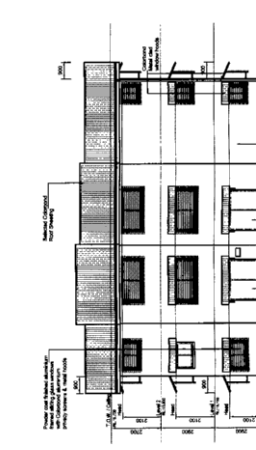
Elevation - Northern - Block A



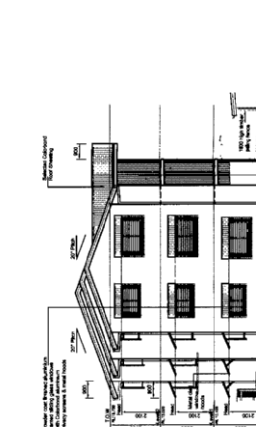
Elevation - Southern - Block B



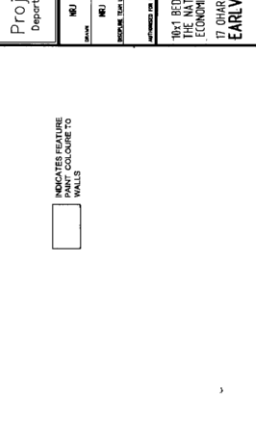
Elevation - Northern - Block B



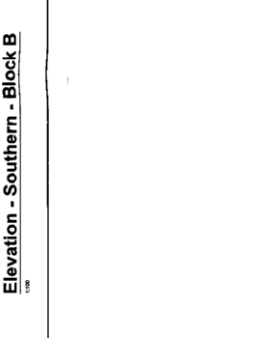
Elevation - Western - Block A



Elevation - O'Hara Street - Block B



Elevation - Western - Block B



Elevation - Southern - Block B

FORM B202 (2004)